



Meeting Date: January 20, 2023

To: Public Works Commission
Lauren Post, Chair
Fady Zoubi, Vice Chair
Lynne Newhouse Segal
Paul Woolford CA

Through: Carla Short, Interim Public Works Director

From: Jane Chan, Public Works Project Architect

Subject: Castro Station Elevator - Contract Award

Director’s Recommendation:

Approve the proposed resolution awarding the Castro Station Elevator Project contract to CLW Builders, Inc. in the amount of \$11,540,000, with a \$115,400 contingency and a contract duration of 600 consecutive calendar days. On July 9, 2021, the activities encompassed under the contract were determined to be exempt from the California Environmental Quality Act (CEQA) under the Class 11 categorical exemption (CEQA Guidelines Section 15311), as described in the CEQA Exemption Determination (Case No. 2018-005547ENV) contained in the Commission’s files for this contract.

Project and Contract Background:

As part of a larger San Francisco Municipal Transportation Agency (SFMTA) system-wide effort to improve accessibility of the MUNI subway system, the Castro Muni Station will undergo several upgrades in and around the station area to improve ADA access to the station and to the transit services. The Castro Station Elevator Project will add a second street-level elevator to the station to provide continuous access and elevator redundancy to the station, trains, and buses. The 4-stop elevator design includes a translucent etched pattern on clear laminated glass façade to provide unobstructed sightline into the elevator cab to improve rider safety. To create safer and ADA compliant paths of travel, the brick paving area around the station entrance at street and concourse levels will be regraded. A portion of the sidewalk on Market Street will be made wider, and new curb ramps will be added at Market and Collingwood Streets. The existing Harvey Milk Plaza lighting will be replaced, and the planting terraces at the concourse level will be replanted, and new fences and bench will be installed.

Solicitation Process:

On April 18, 2022, the project was advertised for Bid with \$12,000,000 as the contract estimate. The Contract Monitoring Division (CMD) set the LBE subcontracting participation requirement at 20%. The City received three bids on August 23, 2022. The Tabulation of Bids and CMD’s

confirmation identified CLW as the apparent low bidder in accordance with the requirements of San Francisco Administrative Code Chapter 14B, following by Rubecon and Trico Construction (Tabulation of Bids attached as Exhibit A). Upon its first review of the submitted bids, the City deemed all three bids responsive.

Trico and Rubecon submitted timely written protests of CLW’s bid. The protests claimed that CLW did not meet the project’s requirements per the bid documents, did not list all subcontractors performing more than ½ of 1% of the work as required by the Subletting and Subcontracting Fair Practices Act, and did not provide all the documentation required by CMD Form 2B. After reviewing the additional documents and testimony provided by CLW and consulting with CMD compliance staff and the City Attorney’s office, on November 10, 2022, Public Works rejected the protests of CLW’s bid.

Public Works determined that CLW is the responsible bidder submitting the lowest responsive bid.

Contract Details:

Contract Title:	PW Castro Station Elevator
Contract Award Amount:	\$11,540,000
Advertised Contract Estimate:	\$12,000,000
Contract Funding Sources (if applicable):	The funding sources include SFMTA General Obligation (GO) Bond funds, SFMTA Operating funds, and Transportation Sustainability Fee (TSF) funds.
Anticipated Project Schedule:	Notice-to-Proceed: March 2023 Construction: March 2023 – November 2024
Contract Duration:	600 consecutive calendar days
Contractor Name:	CLW Builders, Inc.
Compliance with Chapter 14B Local Business Enterprise Ordinance	The Contract Monitoring Division (CMD) established a Local Business Enterprise (LBE) subcontracting requirement of 20% for this contract. The contractor committed to LBE participation of 20.80% in its bid submittal, and San Francisco Public Works received CMD’s recommendation to award. (CMD Award Memo attached as <u>Exhibit B</u>).
Environmental Determination (if applicable):	California Environment Quality Act (CEQA) Determination issued by the Planning Department on July 9, 2021 (Case No. 2018-005547ENV, attached as Exhibit C) finding the project is categorically exempt under CEQA Guidelines Sec. 15311 as the

	proposed new elevator is an accessory structure to the Castro Muni Station.
Other Compliance	N/A
Related Commission Actions:	N/A.
Additional Information:	N/A
Attachments:	Attachment A: Tabulation of Bids Attachment B: CMD Award Memo Attachment C: CEQA Determination Attachment D: Resolution Attachment E: Presentation

PUBLIC WORKS COMMISSION
CITY AND COUNTY OF SAN FRANCISCO

RESOLUTION NO. _____

WHEREAS, As part of a larger San Francisco Municipal Transportation Agency (SFMTA) system-wide effort to improve accessibility of the Muni subway system, the Castro Muni Station in San Francisco, California will undergo several upgrades in and around the Station area to improve ADA access to the Station and to the transit services; and

WHEREAS, On July 9, 2021, the Planning Department issued a determination (Case Number 2018-005547ENV) that the construction of a new four-stop street level elevator to the Castro Muni Station to provide continuous access to the station, trains, and buses as part of the SFMTA – Castro Station Accessibility Improvement Project (as further described herein, the “Castro Station Elevator Project”) is categorically exempt from the California Environmental Quality Act (“CEQA”) under the Class 11 exemption, CEQA Guidelines Section 15311 (“CEQA Determination” attached to this resolution); and

WHEREAS, The Castro Station Elevator Project will add a new street level elevator to the station to provide continuous access to the station, trains, and buses; feature a four-stop elevator design including a translucent etched pattern on clear laminated glass façade to provide unobstructed sightline into the elevator cab to improve rider safety; create safer and ADA compliant paths of travel by re-grading the brick paving area around the station entrance at street and concourse levels; widen a portion of the sidewalk on Market Street and add new curb ramps at Market and Collingwood Streets; replace the existing Harvey Milk Plaza lighting, replant the planting terraces at the concourse level; and install new fences and a bench; and

WHEREAS, The Contract Monitoring Division (CMD) established a Local Business Enterprise (LBE) subcontracting requirement of 20% for this contract. The contractor committed to LBE participation of 20.80% in its bid submittal, and San Francisco Public Works received CMD’s recommendation to award; and

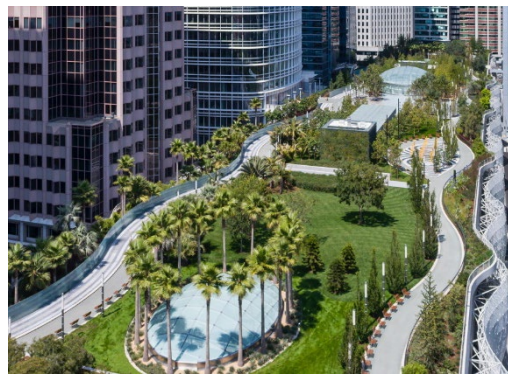
WHEREAS, The project’s advertised contract estimate was \$12,000,000.00, and the contract duration is six hundred (600) consecutive calendar days from NTP to substantial completion; and

WHEREAS, The Department advertised the project on April 18, 2022 and received three (3) bids on August 24, 2022. In November 2022, Public Works Contract Administration and project staff determined that CLW Builders, Inc. was the responsible bidder who submitted the lowest responsive bid; now, therefore, be it

RESOLVED, That this Commission hereby awards San Francisco Public Works Construction Contract, Castro Station Elevator to CLW Builders, Inc. in the amount of \$11,540,000 with a contract duration of 600 consecutive calendar days and a \$115,400 contingency to perform ADA access improvements to the area in and around the Castro Muni Station and to construct a new four-stop street level elevator at the Castro Muni Station.

I hereby certify that the foregoing resolution was adopted by the Public Works Commission at its meeting of _____.

Commission Affairs Manager, Public Works Commission



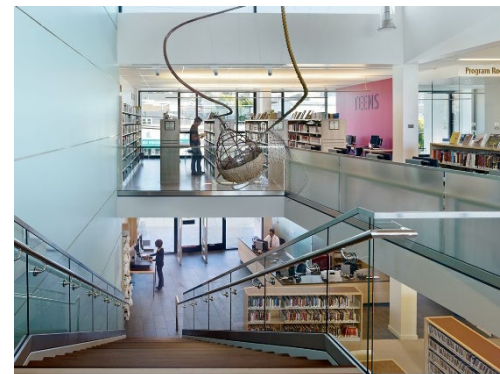
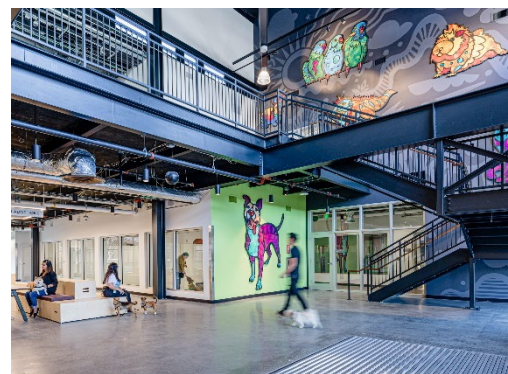
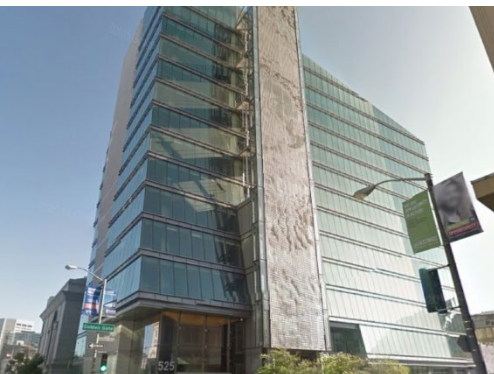
January 20, 2022

Castro Station Elevator

Market and Castro streets, SW corner

Jane Chan

Public Works Project Architect



Recommend Commission:

**Award San Francisco Public Works Contract:
Castro Station Elevator Project**

Amount

\$11,540,000

Duration

600 calendar days

Contractor

CLW Builders, Inc.

Reason:

Improve access to Castro Station and Muni Metro

Project Overview & Scope of Work

Overview

Client: SF Municipal Transportation Agency (SFMTA)

Contract: Castro Station Elevator

Design Lead: Public Works, Building Design and Construction Bureau of Architecture (BOA)

Project Scope of Work:

- Site analysis** and preliminary reporting
- Design and development** of a 4-stop elevator and ADA improvements to the surrounding site
- Preparation** of construction documents
- Obtain approvals** from the San Francisco Arts Commission, Planning and BART
- Construction administration** services



Project Overview & Scope of Work

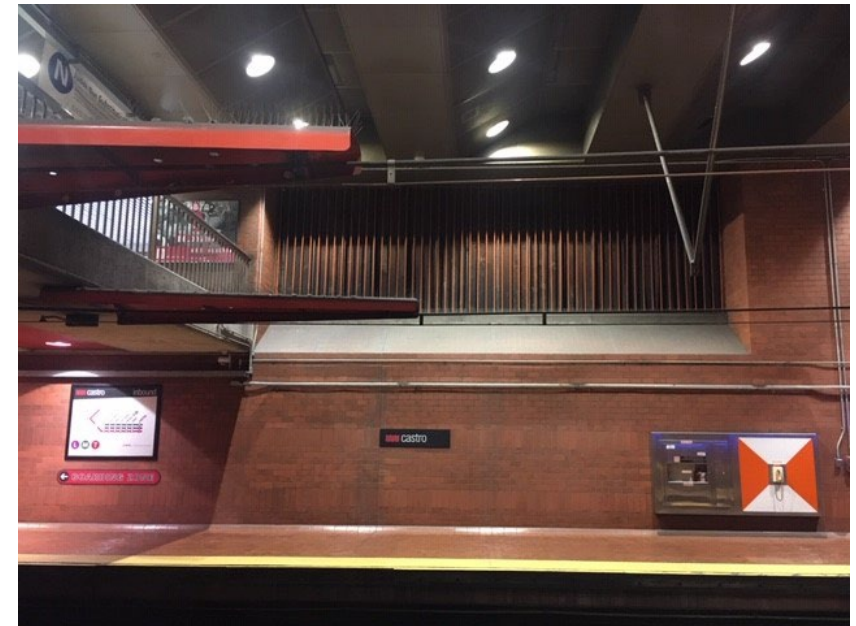
Existing Conditions



Castro Street and Upper Plaza



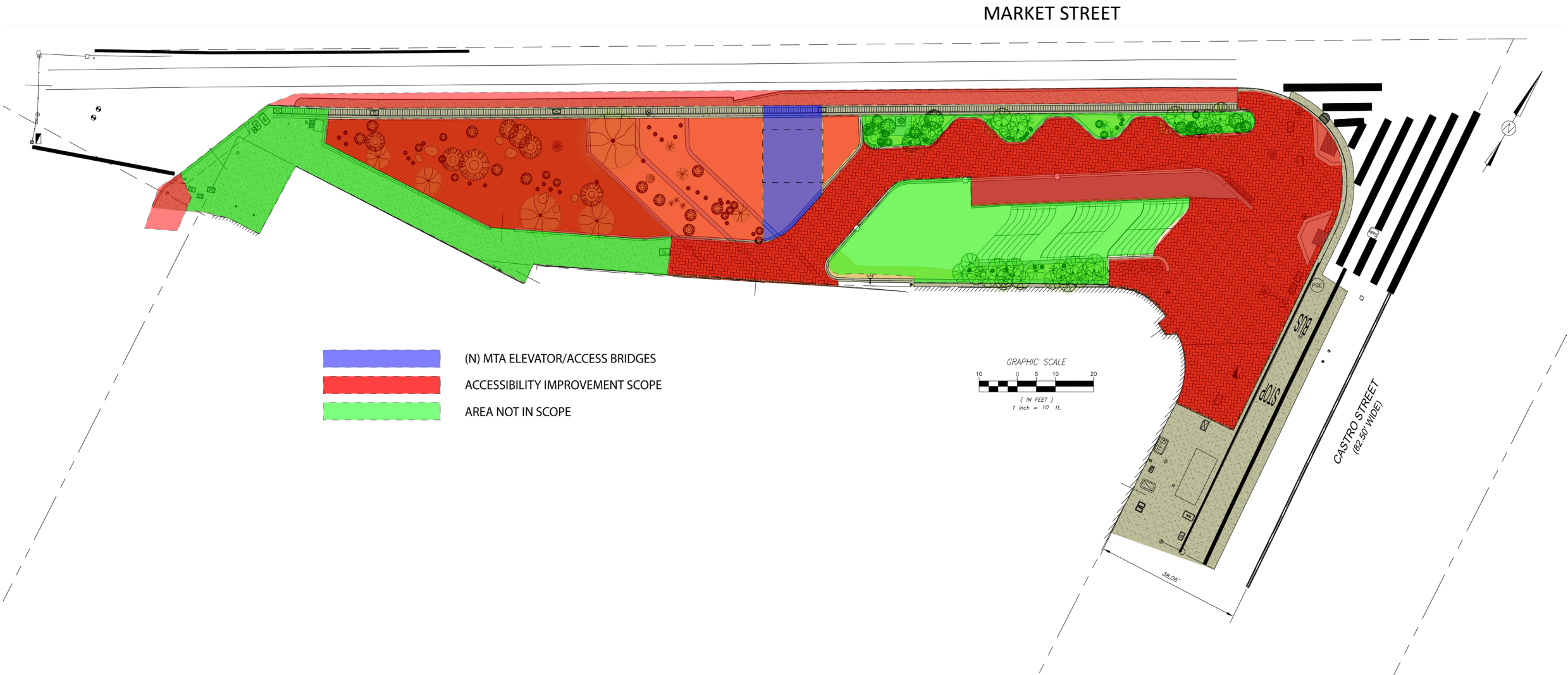
Concourse and Lower Plaza



Station Platform

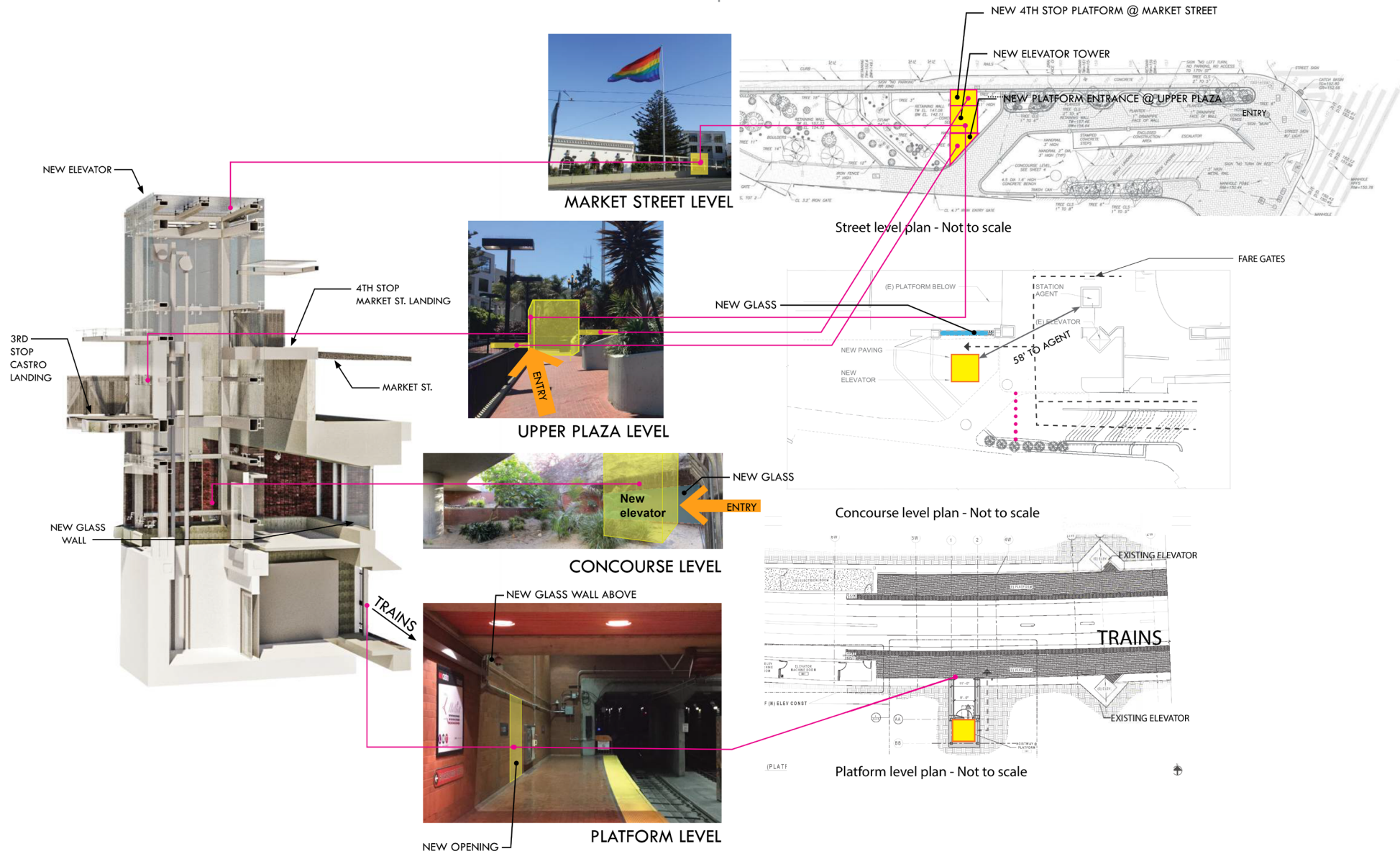
Project Overview and Scope of Work

Street-Level Site Plan



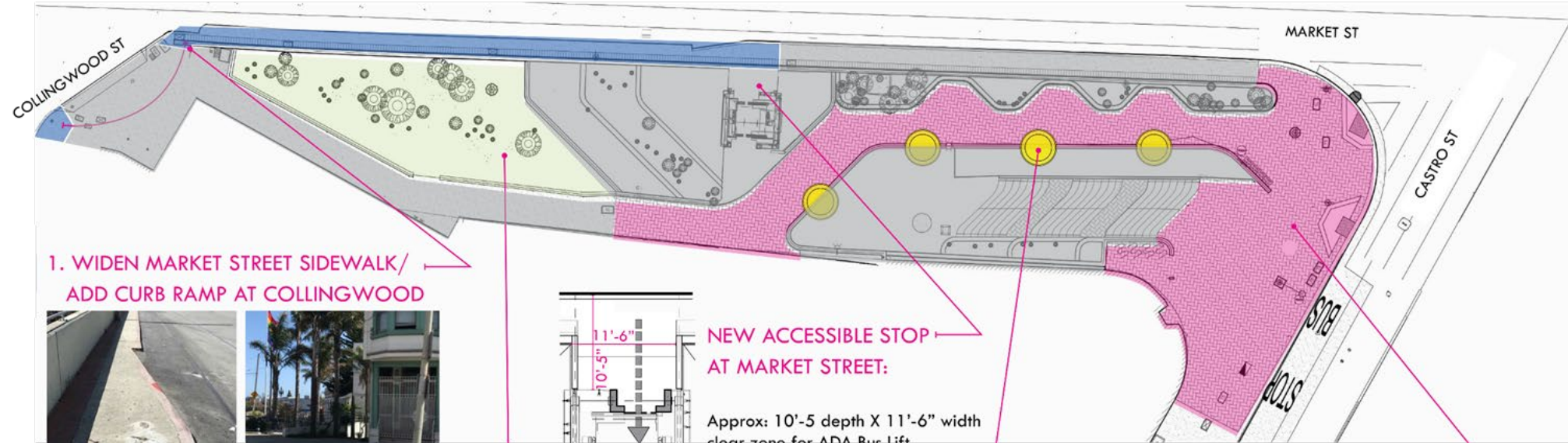
Project Overview and Scope of Work

New Elevator



Project Overview and Scope of Work

Street-Level Improvements



1. WIDEN MARKET STREET SIDEWALK/ ADD CURB RAMP AT COLLINGWOOD

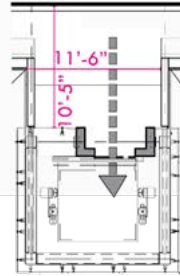


Widen Market Street sidewalk to match adjacent width and accommodate Muni ADA lift deployment at elevator. Add new curb ramp at Market and Collingwood Streets.

2. REPLANT STREET LEVEL PLANTING AREA



Plant with drought-tolerant and low-maintenance species. Swaying plants soften the station's structure, while strategically placed agaves provide bold focal points and improve security.



NEW ACCESSIBLE STOP AT MARKET STREET:

Approx: 10'-5 depth X 11'-6" width
clear zone for ADA Bus Lift
(8' X 8' required by SFMTA)

3. REPLACE PLAZA LIGHT FIXTURES



Replace existing pole top light fixtures with "Torres" fixtures by Landscape Forms.

4. REGRADE AND REPLACE PLAZA PAVING



Replace existing brick with sparkle grain integral color concrete to match Castro streetscape paving

Project Overview and Scope of Work

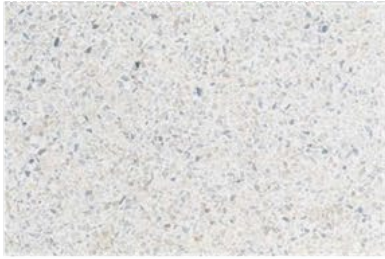
Concourse-Level Improvements

1. REPLANT TERRACES AND CONCOURSE



Plant with drought-tolerant and low maintenance species that tolerate a range of sun and shade. Swaying plants soften the station's structure, while strategically placed agaves provide bold focal points and improve security.

2. PROVIDE NEW PAVING



Replace existing paving at station entrance to correct drainage issues. Provide new paving at elevator "garden room." Paving to be a non-slip terrazo-style aggregate concrete paving in a warm tone to coordinate with interior station floor material.

3. REPLACE FENCING AND INTERPRETIVE SIGNAGE



Porcelain example: Unity Plaza Stairs

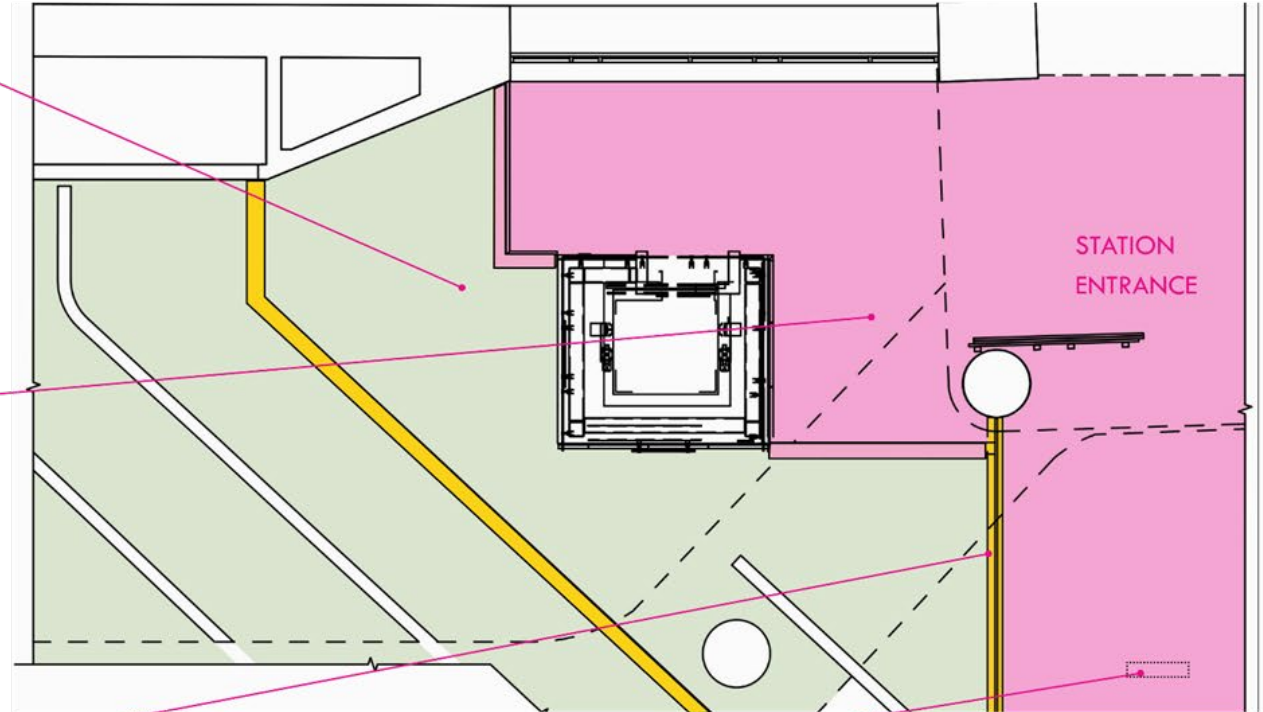
Replace and realign existing fencing to improve station security. Coordinate new fence detailing and material with existing station gate for a unified appearance.

Re-fabricate existing interpretive signage in durable photo-enamel porcelain. Mount at same height and location on new fence with custom metal frame for a more integrated appearance.

4. REPLACE BENCH

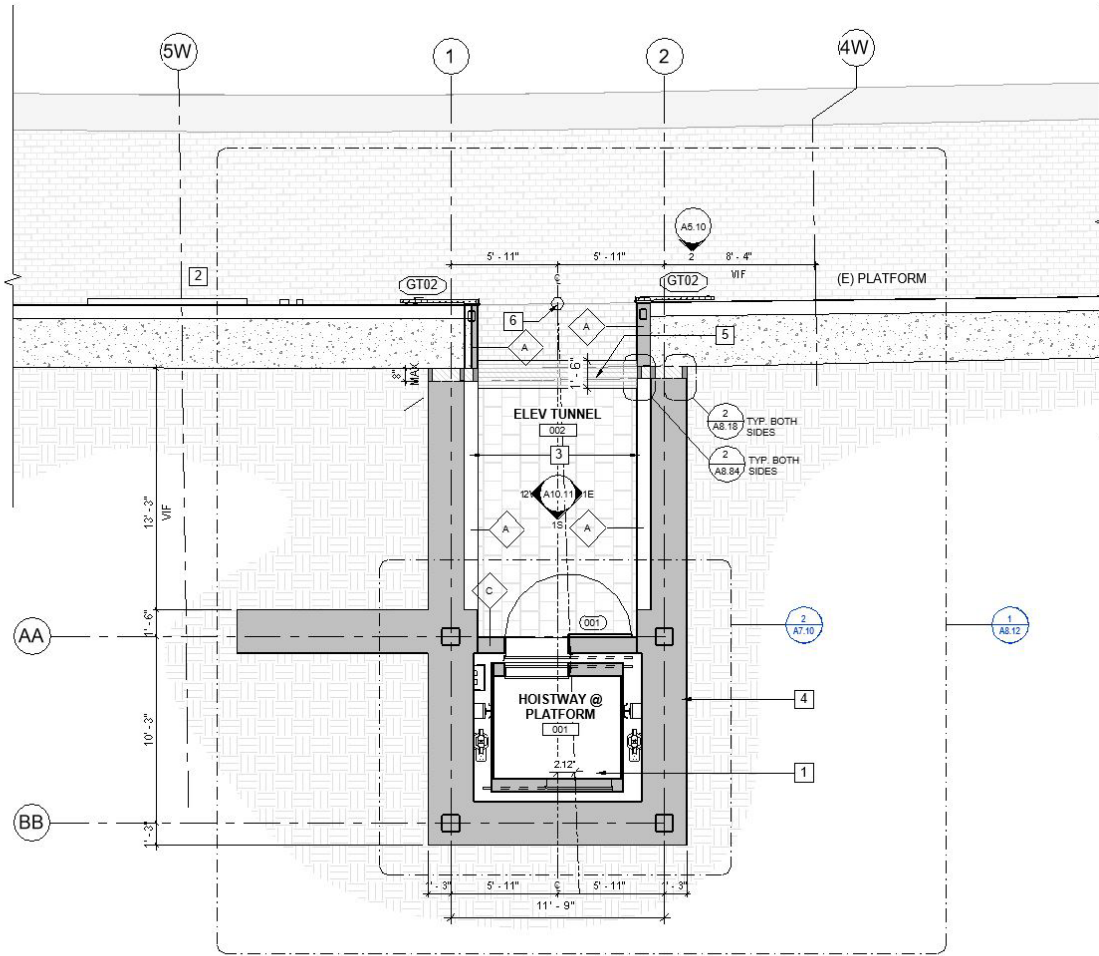


Replace existing damaged commemorative bench with a new metal bench.

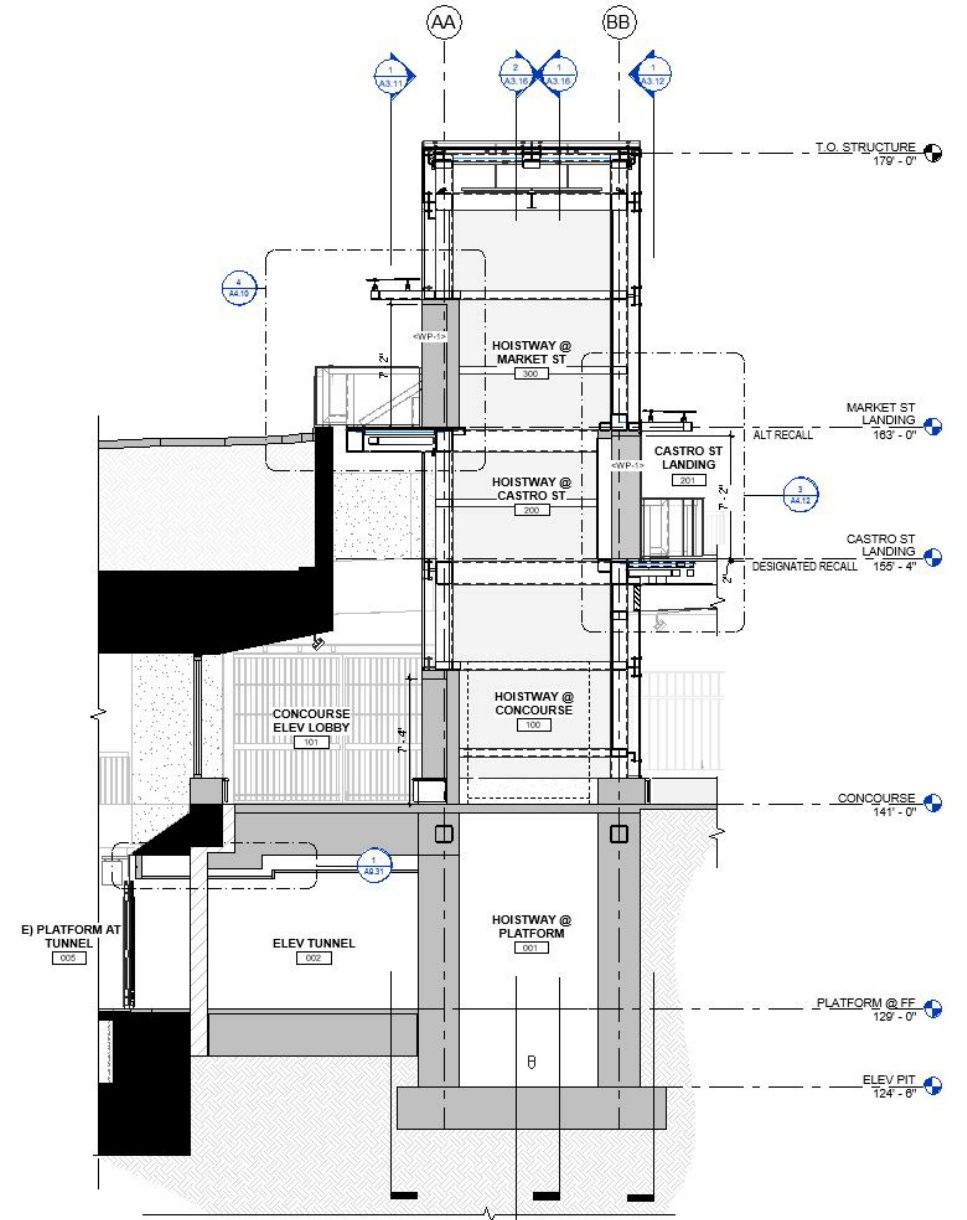


Project Overview and Scope of Work

Platform-Level Improvements



1 PLATFORM LANDING - (N) ELEVATOR TUNNEL - ENLARGED PLAN
1/4" = 1'-0"



2 SECTION A-A THROUGH HOISTWAY - EAST
1/4" = 1'-0"



Key Milestone Dates

Key Schedule Information / Milestones	Dates
Project Advertised	April 18, 2022
Bid Opened	August 23, 2022
Bid Protest Resolved	November 30, 2022
Anticipated Notice-to-Proceed	March 2023
Anticipated Construction Start	March 2023
Anticipated Project Substantial Completion	November 2024
Anticipated Final Completion	February 2025

Project Images



Castro Street Level (FRONT)

Project Images



Market Street Level



Market Street Level



Castro Street Level (BACK)



Recommend Commission:

**Award San Francisco Public Works Contract:
Castro Station Elevator Project**

Amount

\$11,540,000

Duration

600 calendar days

Contractor

CLW Builders, Inc.

Reason:

Improve access to Castro Station and Muni Metro

Questions?



sfpublicworks.org

**City and County of San Francisco
Department of Public Works
TABULATION OF BIDS
PLA**

SOURCING ID: 0000005517

CONTRACT TITLE: PW CASTRO STTN ELEV

FULL TITLE: CASTRO STATION ELEVATOR

BIDS RECEIVED: August 24, 2022

<u>BIDDERS (in the order received & opened):</u>	<u>LBE Status Claimed</u>	<u>Total Bid Price</u>
CLW Builders, Inc.	Micro-LBE / Small-LBE / SBA-LBE 2%	\$11,540,000.00
Trico Construction	Micro-LBE / Small-LBE / SBA-LBE 2%	\$19,666,353.00
Rubecon General Contracting, Inc.	Micro-LBE / Small-LBE / SBA-LBE 2%	\$19,057,335.00
	Average Bid:	\$16,754,562.67
	Engineer's Estimate:	\$12,000,000.00
	% of Engineer's Estimate:	140%
	% of Engineer's Estimate vs. Low Bid Received	96%

[Redacted] = *Indicates a correction of the bid price after review.*

cc:	Ben Wu	Carla Short	Albert Ko	Ron Alameida
	Selormey Dzikunu	Fernando Cisneros	K2 Systems	All Bidders
	Ivan Oldenkamp	Patrick Rivera	Nicolas Huff	
	Ed Yee	Cyril Velasquez	Raymond Woo	

For complete subcontractor listings, check: <https://bidopportunities.apps.sfdpw.org/CaseLoad/Details/2360>

Sourcing ID: 000005517
 Contract Title: PW CASTRO STTN ELEV
 Full Title: CASTRO STATION ELEVATOR
 Bid Date: August 24, 2022

BID ITEMS	QUANTITIES	UNIT	UNIT PRICE	CLW Builders, Inc.		Trico Construction		Rubecon General Contracting, Inc.	
				EXTENSION	UNIT PRICE	EXTENSION	UNIT PRICE	EXTENSION	UNIT PRICE
1	---	LS	---	\$5,623,454.00	---	\$1,623,461.00	---	\$2,486,900.00	---
2	---	LS	---	\$496,000.00	---	\$1,024,671.00	---	\$602,200.00	---
3	---	LS	---	\$450,000.00	---	\$1,425,407.00	---	\$799,100.00	---
4	---	LS	---	\$77,000.00	---	\$144,232.00	---	\$139,500.00	---
5	---	LS	---	\$55,000.00	---	\$262,461.00	---	\$271,100.00	---
6	---	LS	---	\$150,000.00	---	\$423,931.00	---	\$84,100.00	---
7	---	LS	---	\$55,000.00	---	\$142,615.00	---	\$40,100.00	---
8	---	LS	---	\$15,000.00	---	\$20,800.00	---	\$53,300.00	---
9	---	LS	---	\$110,438.00	---	\$122,439.00	---	\$57,000.00	---
10	---	LS	---	\$46,200.00	---	\$91,231.00	---	\$536,900.00	---
11	---	LS	---	\$12,000.00	---	\$84,614.00	---	\$217,600.00	---
12	---	LS	---	\$33,883.00	---	\$67,154.00	---	\$63,600.00	---
13	---	LS	---	\$45,910.00	---	\$119,945.00	---	\$79,600.00	---
14	---	LS	---	\$55,000.00	---	\$96,247.00	---	\$50,900.00	---
15	---	LS	---	\$85,000.00	---	\$437,462.00	---	\$194,100.00	---
16	---	LS	---	\$310,000.00	---	\$471,102.00	---	\$397,600.00	---
17	---	LS	---	\$50,200.00	---	\$241,462.00	---	\$194,100.00	---
18	---	LS	---	\$7,300.00	---	\$115,462.00	---	\$231,100.00	---
19	---	LS	---	\$8,510.00	---	\$50,000.00	---	\$84,100.00	---
20	---	LS	---	\$6,000.00	---	\$60,000.00	---	\$34,600.00	---
S-1	---	LS	---	\$23,880.00	---	\$117,765.00	---	\$128,100.00	---
S-2	---	LS	---	\$10,200.00	---	\$65,731.00	---	\$227,100.00	---
S-3	---	LS	---	\$28,200.00	---	\$96,327.00	---	\$799,100.00	---
S-4	---	LS	---	\$1,100,000.00	---	\$3,935,879.00	---	\$3,142,600.00	---
S-5	---	LS	---	\$163,500.00	---	\$398,850.00	---	\$128,900.00	---
S-6	---	LS	---	\$49,600.00	---	\$658,147.00	---	\$570,300.00	---
S-7	---	LS	---	\$860,000.00	---	\$2,917,307.00	---	\$2,038,300.00	---
S-8	---	AL	---	\$20,000.00	---	\$20,000.00	---	\$20,000.00	---
R-1	300	SF	\$25.00	\$7,500.00	\$20.00	\$6,000.00	\$15.50	\$4,650.00	---
R-2	5	US SHORT TON	\$2,000.00	\$10,000.00	\$2,700.00	\$13,500.00	\$2,200.00	\$11,000.00	---
R-3	220	SF	\$25.00	\$5,500.00	\$41.00	\$9,020.00	\$46.50	\$10,230.00	---
R-4	2,500	SF	\$20.00	\$50,000.00	\$20.00	\$50,000.00	\$44.00	\$110,000.00	---
R-5	335	LF	\$65.00	\$21,775.00	\$85.00	\$28,475.00	\$99.00	\$33,165.00	---
R-6	40	LF	\$90.00	\$3,600.00	\$115.00	\$4,600.00	\$186.00	\$7,440.00	---
R-7	2	EA	\$7,000.00	\$14,000.00	\$5,623.00	\$11,246.00	\$4,200.00	\$8,400.00	---
R-8	---	LS	---	\$6,700.00	---	\$15,000.00	---	\$6,100.00	---
R-9	---	LS	---	\$15,000.00	---	\$266,154.00	---	\$707,800.00	---
R-10	6	EA	\$250.00	\$1,500.00	\$800.00	\$4,800.00	\$800.00	\$4,800.00	---
R-11	1	EA	\$6,000.00	\$6,000.00	\$800.00	\$800.00	\$29,700.00	\$29,700.00	---
LA-1	---	LS	---	\$22,000.00	---	\$96,430.00	---	\$78,600.00	---
LA-2	---	LS	---	\$105,000.00	---	\$25,000.00	---	\$51,100.00	---
LA-3	60	CY	\$320.00	\$19,200.00	\$146.00	\$8,760.00	\$42,600.00	\$2,556,000.00	---
LA-4	---	LS	---	\$107,000.00	---	\$122,864.00	---	\$150,100.00	---
LA-5	---	LS	---	\$60,000.00	---	\$89,270.00	---	\$100,600.00	---
LA-6	4,500	SF	\$44.00	\$198,000.00	\$29.00	\$130,500.00	\$26.50	\$119,250.00	---
LA-7	1,800	SF	\$44.00	\$79,200.00	\$34.00	\$61,200.00	\$31.50	\$56,700.00	---
LA-8	---	LS	---	\$62,000.00	---	\$133,231.00	---	\$29,100.00	---
LA-9	---	LS	---	\$46,150.00	---	\$141,275.00	---	\$55,600.00	---
LA-10	1	EA	\$2,000.00	\$2,000.00	\$5,000.00	\$5,000.00	\$84,100.00	\$84,100.00	---
LA-11	1	EA	\$5,500.00	\$5,500.00	\$5,200.00	\$5,200.00	\$34,800.00	\$34,800.00	---
LA-12	---	LS	---	\$188,100.00	---	\$108,231.00	---	\$84,100.00	---
LA-13	---	LS	---	\$50,000.00	---	\$88,113.00	---	\$139,100.00	---
G-1	---	LS	---	\$400,000.00	---	\$2,899,982.00	---	\$806,000.00	---
G-2	---	AL	---	\$62,000.00	---	\$62,000.00	---	\$62,000.00	---
G-3	---	AL	---	\$45,000.00	---	\$45,000.00	---	\$45,000.00	---
TOTAL BID PRICE:				\$11,540,000.00		\$19,666,353.00		\$19,057,335.00	

Indicates a correction of the bid price after review.



CONTRACT MONITORING DIVISION OFFICE OF THE CITY ADMINISTRATOR



London N. Breed, Mayor
Carmen Chu, City Administrator

Romulus Asenloo, Director

MEMORANDUM

Date: November 24, 2022

To: Ben Washington, SF Public Works
Jane Chan, Project Manager, SF Public Works

From: Ivan Oldenkamp, Contract Monitoring Division

Subject: Sourcing ID: 0000005517 Castro Station Elevator – CMD Review Memo Rev. 2

The Contract Monitoring Division (“CMD”) has reviewed the bids submitted for the above referenced project, applied the appropriate bid discounts, and determined responsiveness to the Chapter 14B pre-award requirements. An LBE subcontracting requirement of 20% was established for this project. We have also reviewed and considered Rubecon’s protest letter dated August 31, 2022. Below is a summary of CMD’s review and findings.

Bidder	LBE Status	Base Bid	LBE Bid Discount	Adjusted Bid with Bid Discount
CLW Builders	SF LBE - MBE (Small)	\$11,540,000.00	10%	\$10,386,000.00
Trico Construction	SF LBE - WBE (Micro)	\$19,666,344.00	10%	\$17,699,709.60
Rubecon General Contracting Inc.	SF LBE - MBE (Micro)	\$19,057,335.00	10%	\$17,151,601.50

CLW Builders, Inc. (“CLW”) is the lowest responsive bidder. CLW satisfied the Good Faith Outreach requirement by exceeding the subcontractor participation requirement by at least 35% by counting its own participation as an LBE prime contractor.

CLW met the 20% LBE subcontracting requirement by listing the following LBE firms for the base bid items:

LBE Subcontractor	Scope of Work	Status	Percent LBE	Listed Amount	Amount Credited	Percent of Work
KM106	Hazmat	SF LBE - MBE (Micro)	100%	\$62,000.00	\$0.00**	0.00%
IBS USA	Structural steel, decorative metal	SF LBE - MBE (Micro)	100%	\$1,010,300.00	\$1,010,300.00	8.75%
Priority Arch Graphics	Signage	SF LBE - WBE (Micro)	100%	\$110,438.00	\$110,438.00	0.96%
D & S Leong	Plumbing, fire sprinkler	SF LBE - MBE (Micro)	100%	\$143,200.00	\$143,200.00	1.24%
All Phase Electrical	Electrical, Fire Alarm	SF LBE - MBE (Micro)	100%	\$698,000.00	\$698,000.00	6.05%
ABCO	HVAC	SF LBE - OBE (Micro)	100%	\$45,910.00	\$41,410.00	0.36%
RK Engineering	Site paving, sidewalk	SF LBE - OBE (Micro)	100%	\$397,075.00	\$397,075.00	3.44%
TOTAL				\$2,466,923.00	\$2,400,423.00	20.80%

** Per CMD Form 6 submitted for KM106, KM106 will be performing work for the Allowance for Environmental Work scope; allowances cannot be used to meet the LBE subcontracting participation requirement at time of bid.

CLW has also listed the following non-LBE subcontractors for base bid items:

Non-LBE Subcontractor	Scope of Work	Status	Listed Amount
Cal Pacific Construction	Concrete, formwork, misc demo	Non-LBE	\$1,365,000.00
Novum Structures	Point supported glazing, storefront	Non-LBE	\$598,000.00
SF Elevator	Elevator	Non-LBE	\$950,000.00
KZ Tile	Tiling	Non-LBE	\$95,500.00
Green Growth	Landscape & irrigation	Non-LBE	\$263,940.00
West Coast Drilling	Micropiles	Non-LBE	\$1,114,000.00
TOTAL			\$4,386,440

Based on the foregoing, CMD has determined that CLW Builders, Inc. has complied with the Chapter 14B pre-award requirements. Should you have any questions, please contact Ivan Oldenkamp at ivan.oldenkamp@sfgov.org



CEQA Exemption Determination

PROPERTY INFORMATION/PROJECT DESCRIPTION

Project Address		Block/Lot(s)
SFMTA - Castro Station Accessibility Improvements Project		
Case No.		Permit No.
2018-005547ENV		
<input type="checkbox"/> Addition/ Alteration	<input checked="" type="checkbox"/> Demolition (requires HRE for Category B Building)	<input checked="" type="checkbox"/> New Construction
<p>Project description for Planning Department approval.</p> <p>The San Francisco Municipal Transportation Agency (SFMTA) proposes to construct a new four-stop elevator on the south side of Market Street at the Castro Muni Station to improve the accessibility of the Muni subway system. The new elevator would be located immediately adjacent to the existing Station within the existing landscaped area at the concourse level. The new elevator would integrate with the existing architectural and structural framework and would consist of an approximately 39-foot-tall (from Concourse Mezzanine Level) elevator tower with a glass canopy. The elevator would include one stop on Market Street, one stop on Harvey Milk Plaza, one stop on the concourse level, and one stop on the platform level of the Station. The Market Street elevator stop/landing would allow disabled passengers from the adjacent Market Street bus stop to directly access the Station via a pedestrian bridge. A ten-foot portion of the existing wall along Market Street would be removed to accommodate the new elevator and landing. The plaza-level elevator stop/landing would also be served by a pedestrian bridge and would create an accessible path from the southwest corner of Market and Castro Streets to the elevator entrance. These proposed features would address the existing non-compliant paths of travel in and around the</p> <p>FULL PROJECT DESCRIPTION ATTACHED</p>		

STEP 1: EXEMPTION TYPE

The project has been determined to be exempt under the California Environmental Quality Act (CEQA).	
<input type="checkbox"/>	Class 1 - Existing Facilities. Interior and exterior alterations; additions under 10,000 sq. ft.
<input type="checkbox"/>	Class 3 - New Construction. Up to three new single-family residences or six dwelling units in one building; commercial/office structures; utility extensions; change of use under 10,000 sq. ft. if principally permitted or with a CU.
<input type="checkbox"/>	<p>Class 32 - In-Fill Development. New Construction of seven or more units or additions greater than 10,000 sq. ft. and meets the conditions described below:</p> <p>(a) The project is consistent with the applicable general plan designation and all applicable general plan policies as well as with applicable zoning designation and regulations.</p> <p>(b) The proposed development occurs within city limits on a project site of no more than 5 acres substantially surrounded by urban uses.</p> <p>(c) The project site has no value as habitat for endangered rare or threatened species.</p> <p>(d) Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality.</p> <p>(e) The site can be adequately served by all required utilities and public services.</p>
<input checked="" type="checkbox"/>	<p>Other _____</p> <p>Class 11 (15311) - ACCESSORY STRUCTURES consists of construction, or placement of minor structures accessory to (appurtenant to) existing commercial, industrial, or institutional facilities.</p>
<input type="checkbox"/>	Common Sense Exemption (CEQA Guidelines section 15061(b)(3)). It can be seen with certainty that there is no possibility of a significant effect on the environment.

**STEP 3: PROPERTY STATUS - HISTORIC RESOURCE
TO BE COMPLETED BY PROJECT PLANNER**

PROPERTY IS ONE OF THE FOLLOWING: (refer to Property Information Map)	
<input checked="" type="checkbox"/>	Category A: Known Historical Resource. GO TO STEP 5.
<input type="checkbox"/>	Category B: Potential Historical Resource (over 45 years of age). GO TO STEP 4.
<input type="checkbox"/>	Category C: Not a Historical Resource or Not Age Eligible (under 45 years of age). GO TO STEP 6.

**STEP 4: PROPOSED WORK CHECKLIST
TO BE COMPLETED BY PROJECT PLANNER**

Check all that apply to the project.	
<input type="checkbox"/>	1. Change of use and new construction. Tenant improvements not included.
<input type="checkbox"/>	2. Regular maintenance or repair to correct or repair deterioration, decay, or damage to building.
<input type="checkbox"/>	3. Window replacement that meets the Department's <i>Window Replacement Standards</i> . Does not include storefront window alterations.
<input type="checkbox"/>	4. Garage work. A new opening that meets the <i>Guidelines for Adding Garages and Curb Cuts</i> , and/or replacement of a garage door in an existing opening that meets the Residential Design Guidelines.
<input type="checkbox"/>	5. Deck, terrace construction, or fences not visible from any immediately adjacent public right-of-way.
<input type="checkbox"/>	6. Mechanical equipment installation that is not visible from any immediately adjacent public right-of-way.
<input type="checkbox"/>	7. Dormer installation that meets the requirements for exemption from public notification under <i>Zoning Administrator Bulletin No. 3: Dormer Windows</i> .
<input type="checkbox"/>	8. Addition(s) that are not visible from any immediately adjacent public right-of-way for 150 feet in each direction; does not extend vertically beyond the floor level of the top story of the structure or is only a single story in height; does not have a footprint that is more than 50% larger than that of the original building; and does not cause the removal of architectural significant roofing features.
Note: Project Planner must check box below before proceeding.	
<input checked="" type="checkbox"/>	Project is not listed. GO TO STEP 5.
<input type="checkbox"/>	Project does not conform to the scopes of work. GO TO STEP 5.
<input type="checkbox"/>	Project involves four or more work descriptions. GO TO STEP 5.
<input type="checkbox"/>	Project involves less than four work descriptions. GO TO STEP 6.

**STEP 5: ADVANCED HISTORICAL REVIEW
TO BE COMPLETED BY PRESERVATION PLANNER**

Check all that apply to the project.	
<input checked="" type="checkbox"/>	<p>1. Reclassification of property status. (Attach HRER Part I)</p> <p><input checked="" type="checkbox"/> Reclassify to Category A <input type="checkbox"/> Reclassify to Category C</p> <p style="margin-left: 40px;">a. Per HRER 07/23/2020 (No further historic review)</p> <p style="margin-left: 40px;">b. Other (specify):</p>
<input type="checkbox"/>	2. Project involves a known historical resource (CEQA Category A) as determined by Step 3 and conforms entirely to proposed work checklist in Step 4.
<input type="checkbox"/>	3. Interior alterations to publicly accessible spaces that do not remove, alter, or obscure character defining features.
<input type="checkbox"/>	4. Window replacement of original/historic windows that are not "in-kind" but are consistent with existing historic character.
<input type="checkbox"/>	5. Façade/storefront alterations that do not remove, alter, or obscure character-defining features.

<input type="checkbox"/>	6. Raising the building in a manner that does not remove, alter, or obscure character-defining features.
<input type="checkbox"/>	7. Restoration based upon documented evidence of a building's historic condition, such as historic photographs, plans, physical evidence, or similar buildings.
<input checked="" type="checkbox"/>	8. Work consistent with the <i>Secretary of the Interior Standards for the Treatment of Historic Properties (Analysis required)</i> : PLEASE SEE ATTACHED
<input type="checkbox"/>	9. Work compatible with a historic district (Analysis required):
<input type="checkbox"/>	10. Work that would not materially impair a historic resource (Attach HRER Part II).
Note: If ANY box in STEP 5 above is checked, a Preservation Planner MUST sign below.	
<input checked="" type="checkbox"/>	Project can proceed with exemption review. The project has been reviewed by the Preservation Planner and can proceed with exemption review. GO TO STEP 6.
Comments (optional):	
Preservation Planner Signature: Justin Greving	

STEP 6: EXEMPTION DETERMINATION
TO BE COMPLETED BY PROJECT PLANNER

<input checked="" type="checkbox"/>	No further environmental review is required. The project is exempt under CEQA. There are no unusual circumstances that would result in a reasonable possibility of a significant effect.	
	Project Approval Action: BART Building Permit Issuance	Signature: Jennifer M McKellar
		07/09/2021
<p>Supporting documents are available for review on the San Francisco Property Information Map, which can be accessed at https://sfplanninggis.org/PIM/. Individual files can be viewed by clicking on the Planning Applications link, clicking the "More Details" link under the project's environmental record number (ENV) and then clicking on the "Related Documents" link. Once signed or stamped and dated, this document constitutes an exemption pursuant to CEQA Guidelines and Chapter 31 of the Administrative Code.</p> <p>In accordance with Chapter 31 of the San Francisco Administrative Code, an appeal of an exemption determination to the Board of Supervisors can only be filed within 30 days of the project receiving the approval action.</p>		

Full Project Description

The San Francisco Municipal Transportation Agency (SFMTA) proposes to construct a new four-stop elevator on the south side of Market Street at the Castro Muni Station to improve the accessibility of the Muni subway system. The new elevator would be located immediately adjacent to the existing Station within the existing landscaped area at the concourse level. The new elevator would integrate with the existing architectural and structural framework and would consist of an approximately 39-foot-tall (from Concourse Mezzanine Level) elevator tower with a glass canopy. The elevator would include one stop on Market Street, one stop on Harvey Milk Plaza, one stop on the concourse level, and one stop on the platform level of the Station.

The Market Street elevator stop/landing would allow disabled passengers from the adjacent Market Street bus stop to directly access the Station via a pedestrian bridge. A ten-foot portion of the existing wall along Market Street would be removed to accommodate the new elevator and landing. The plaza-level elevator stop/landing would also be served by a pedestrian bridge and would create an accessible path from the southwest corner of Market and Castro Streets to the elevator entrance. These proposed features would address the existing non-compliant paths of travel in and around the Station.

Construction would take approximately 20 months and would consist of installation of shoring, excavation of elevator pit and tunnel, installation of subgrade waterproofing system, forming and pouring of structural concrete footings, erection of steel structural framing, installation of mechanical and electrical infrastructure, installation of the exterior glass cladding, and installation of the elevator machinery, cab and components. In parallel, the path of travel to the new elevator would include the removal of the existing brick walkway surface, re-grading of sub grade and installation of new walkway surface at new ADA compliant slope. Approximately 16,753 cubic feet (620 cubic yards) of soil would be removed during excavation of the elevator pit and the connecting tunnel to the inbound platform (29.1 feet in length, 30.3 feet in width, and 19 feet in depth).

The proposed project would be constructed by Public Works and would implement Public Works Standard Construction Measure 2 (Air Quality) and Public Works Standard Construction Measure 9 (Cultural Resources) as part of the project. Public Works Standard Construction Measure 2 (Air Quality) would require the project to comply with the city's Construction Dust Control Ordinance and Clean Construction Ordinance. Public Works Standard Construction Measure 9 (Cultural Resources) would require the project to implement the following measures to protect in place two existing Path of Gold Light Standards in the project vicinity during construction activities: (1) the project plan set must identify each Path of Gold Light Standard as requiring physical protection; (2) the project plan set must establish environmentally sensitive areas (ESA) radii around each Path of Gold Light Standard; (3) a plan sheet showing the detail of the required physical protection of each Path of Gold Light Standard must be included in the plan set; and (4) the required physical protection of the Path of Gold Light Standards must be constructed and installed to the satisfaction of the Public Works Regulatory Affairs representative overseeing implementation of Public Works Standard Construction Measure 9 (Cultural Resources) before construction activities in the immediate vicinity of the Path of Gold Light Standards may commence.

SEE ATTACHED FULL PROJECT DESCRIPTION FOR MORE DETAIL.

Step 2: Environmental Screening Comments

The proposed project includes Public Works Standard Construction Measure 2 (Air Quality) as part of the project, which would require the project to comply with the city's Construction Dust Control Ordinance and Clean Construction Ordinance. Therefore, air quality impacts would be less than significant.

On 7/9/2021, the San Francisco Public Works' Site Assessment and Remediation (SAR) section evaluated the proposed project and determined that the Maher Ordinance does not apply.

On May 8, 2018 Planning Department archeologists concluded there is low sensitivity for prehistoric archeological sites in this area. Although the project area was developed by at least the early 1850s, the proposed excavation would occur beneath the existing Castro Station and is likely below historic-period features based on boring log from station plans (1972) and archeological review of nearby projects. In 2020, Planning Department archeologists evaluated a revised project with 600 cubic feet of excavation and drilled piers to 70 feet below existing grade and concluded less-than-significant archeological effects due to no sensitivity for buried archeological resources. On April 8, 2021, Planning Department archeological staff reviewed the currently proposed project, which includes 620 cubic yards of excavation over a larger area and depth (than the project proposed in 2020) and drilled piers to 70 feet below existing grade, and concluded the project would have less-than-significant archeological effects due to the aforementioned reasons.

Step 5: #8 Work Consistent With the Secretary of the Interior Standards for the Treatment of Historic Properties Analysis

On July 23, 2020, Planning Department historic preservation staff determined that Harvey Milk Plaza is (1) individually eligible for listing in the California Register under Criterion 1 as a gathering place of exceptionally important vigils and marches, and (2) a contributor to the California Register-eligible Eureka Valley/Castro Street Commercial Historic District.

On June 23, 2021, Planning Department historic preservation staff determined that the proposed project would not have a significant adverse impact on the individual historic resource (Harvey Milk Plaza) or the Eureka Valley/Castro Street Commercial Historic District. Additionally, historic preservation staff determined that the project would not adversely effect two Path of Gold Light Standards (which contribute to Article 10 Landmark #200) because the project would implement Public Works Standard Construction Measure 9 (Cultural Resources), which would require the project to protect in place the two existing Path of Gold Light Standards.

For more information, see the full project description below and historic resource evaluation responses part I and II under Planning Record No.2018-005547ENV.

STEP 7: MODIFICATION OF A CEQA EXEMPT PROJECT

TO BE COMPLETED BY PROJECT PLANNER

In accordance with Chapter 31 of the San Francisco Administrative Code, when a California Environmental Quality Act (CEQA) exempt project changes after the Approval Action and requires a subsequent approval, the Environmental Review Officer (or his or her designee) must determine whether the proposed change constitutes a substantial modification of that project. This checklist shall be used to determine whether the proposed changes to the approved project would constitute a "substantial modification" and, therefore, be subject to additional

MODIFIED PROJECT DESCRIPTION

Modified Project Description:

DETERMINATION IF PROJECT CONSTITUTES SUBSTANTIAL MODIFICATION

Compared to the approved project, would the modified project:

- | | |
|--------------------------|--|
| <input type="checkbox"/> | Result in expansion of the building envelope, as defined in the Planning Code; |
| <input type="checkbox"/> | Result in the change of use that would require public notice under Planning Code Sections 311 or 312; |
| <input type="checkbox"/> | Result in demolition as defined under Planning Code Section 317 or 19005(f)? |
| <input type="checkbox"/> | Is any information being presented that was not known and could not have been known at the time of the original determination, that shows the originally approved project may no longer qualify for the exemption? |

If at least one of the above boxes is checked, further environmental review is required

DETERMINATION OF NO SUBSTANTIAL MODIFICATION

- | | |
|--------------------------|---|
| <input type="checkbox"/> | The proposed modification would not result in any of the above changes. |
|--------------------------|---|

If this box is checked, the proposed modifications are exempt under CEQA, in accordance with prior project approval and no additional environmental review is required. This determination shall be posted on the Planning Department website and office and mailed to the applicant, City approving entities, and anyone requesting written notice. In accordance with Chapter 31, Sec 31.08j of the San Francisco Administrative Code, an appeal of this determination can

Planner Name:

Date:



Date: March 26, 2021
To: Jennifer McKellar, San Francisco Planning Department
Jennifer.Mckellar@sfgov.org, 628.652.7563
From: Tess Kavanagh, San Francisco Municipal Transportation Agency
Tess.Kavanagh@sfmta.com, 415.646.2428
Through: Forrest Chamberlain, San Francisco Municipal Transportation Agency
Forrest.Chamberlain@sfmta.com, 415.646.2898
Subject: Castro Station Accessibility Improvements (Planning Record No. 2018-005547ENV)

Summary

The San Francisco Municipal Transportation Agency (SFMTA) proposes to construct a new four-stop elevator on the south side of Market Street at the Castro Muni Station to improve the accessibility of the Muni subway system. The project would also include creating an accessible path from the southwest corner of Market and Castro Streets to the Plaza-level elevator entry.

Existing Conditions

The Castro Muni Station lies in the heart of the Castro neighborhood providing public transportation access to residents and visitors alike. The Station is an underground MUNI Metro Station at the intersection of Market, Castro, and 17th streets in the Castro neighborhood (Figure1). It is part of the Market Street Subway system that runs underneath Market Street for light rail streetcars. The Station is a two-level underground structure of concrete and steel construction built between 1973 and 1980. The lower Platform Level consists of two platforms on the north and south sides of the tracks. The upper Concourse Mezzanine Level overlooks the tracks and platforms, and contains the fare gates and Station agent booth. The Concourse extends to Harvey Milk Plaza on the southwest corner of Market and Castro Streets. There are two entry points from street level down to the Station. There is only one stairway on each side of Market Street leading down to the Station. One of these entrances is located in Harvey Milk Plaza, and the other is on the northwest corner of Market and 17th Streets. There is only one elevator that connects the Station to street level at the north entry point. The existing path of travel to and from this elevator is not in compliance with accessibility standards. There are two existing Path of Gold Light Standards within the project site: one at the southwest corner of Market and Castro streets and one on the south side of Market Street just east of Collingwood Street. These Path of Gold Light Standards are two of 327 Path of Gold Light Standards, which contribute to Article 10 Landmark #200.

Proposed Project – Elevator

The proposed project would construct a new four-stop elevator on the south side of Market Street at the Castro Muni Station to improve the accessibility of the Muni subway system. The new elevator structure would integrate with the existing architectural and structural framework and would consist of an approximately 39-foot-tall (from Concourse Mezzanine Level) elevator tower with a glass

canopy. The elevator would include one stop on Market Street, one stop on Harvey Milk Plaza, one stop on the concourse level, and one stop on the platform level¹ of the Station.

The Market Street elevator stop/landing would allow disabled passengers from the adjacent Market Street bus stop to directly access the Station via a pedestrian bridge. A ten-foot portion of the existing wall along Market Street would be removed to accommodate the new elevator and landing. The plaza-level elevator stop/landing would also be served by a pedestrian bridge and would create an accessible path from the southwest corner of Market and Castro Streets to the elevator entrance.

These proposed features would make the paths of travel in and around the Station compliant with the Americans with Disabilities Act (ADA).



Figure 1 – Castro Station Footprint (underground extent outlined in red)

Proposed Transportation Elements– Pedestrian Facilities

Pedestrian conditions would improve along the existing paths of travel from the southwest corner of Castro & Market streets to the new elevator entrance at the plaza level of the Station. As part of the project the existing brick pathway paving would be removed and replaced to improve slopes, both

¹ For typical day to day operation, the platform level of the new elevator will be locked off and not accessible to riders. Only during emergency use, such as when the existing in bound platform elevator is out of service, will the platform level of the new elevator be activated. The main objective of the new elevator is to improve access from street levels at Castro and Market to the concourse level of the station.

cross wise and in the direction of travel, to meet ADA requirements. New concrete paving would consist of sparkle grain integral color concrete to match the existing paving of Castro Street. New pathway lighting would also be provided to improve visibility and safety for pedestrians as they access the new elevator. Except for the Path of Gold Light Standards, existing light fixtures and poles would be replaced with new light fixtures and poles.

A portion of the adjacent Market Street sidewalk between Collingwood Street and the new elevator stop would be expanded to match the six-foot width of the remaining Market Street sidewalk between the new elevator stop and Castro Street. Two new ADA accessible curb ramps would be constructed at the adjacent intersections of Collingwood Street at the western entrance to the Station. A portion of the Concourse level of the Station would be repaved leading from the existing staircase in Harvey Milk Plaza to the proposed Concourse level elevator stop.

Proposed Excavation and Soil Disturbance

The new elevator would be located immediately adjacent to the existing Station within the existing landscaped area at the concourse level. Approximately 16,753 cubic feet (620 cubic yards) of soil would be removed during excavation of the elevator pit and the connecting tunnel to the inbound platform (29.1 feet in length, 30.3 feet in width, and 19 feet in depth, as shown in Figure 2).

According to preliminary geotechnical investigation results, the base of the elevator will be founded on a thick concrete mat.² Depending on the final results of the geotechnical investigation, the elevator foundation may require drilled pier footings which would extend down 70 feet from existing grade. This soil was previously disturbed during construction of Castro Station.

² ENGEO Terra Engineers, Inc, *New Elevator for Castro Muni Train Station, San Francisco, CA, Geotechnical Investigation Report (Draft)*, September 17, 2019.

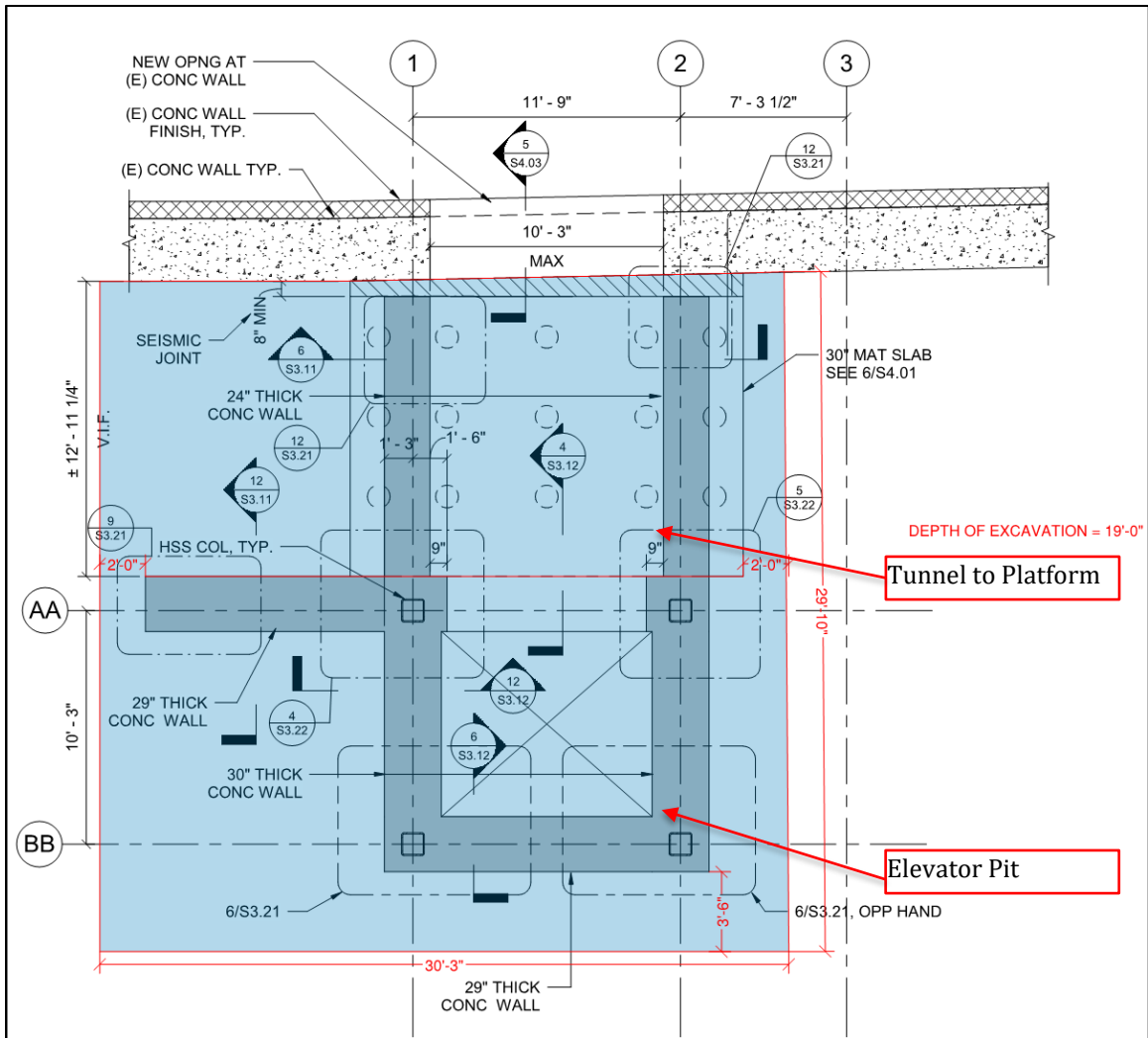


Figure 2 – Elevator Pit and Tunnel to Platform Excavation Diagram

Construction

Construction would take approximately 20 months and would consist of installation of shoring, excavation of elevator pit and tunnel, installation of subgrade waterproofing system, forming and pouring of structural concrete footings, erection of steel structural framing, installation of mechanical and electrical infrastructure, installation of the exterior glass cladding, and installation of the elevator machinery, cab and components. In parallel, the path of travel to the new elevator would include the removal of the existing brick walkway surface, re-grading of sub grade and installation of new walkway surface at new ADA compliant slope. The construction zone would be barricaded to protect the site from intrusion. Access to the Station from the plaza would be maintained throughout the construction period.

The proposed project would be constructed by San Francisco Public Works and would implement Public Works Standard Construction Measure 2 (Air Quality) and Public Works Standard Construction Measure 9 (Cultural Resources) as part of the project. Public Works Standard Construction Measure 2 (Air Quality) would require the project to comply with the city's Construction Dust Control Ordinance and Clean Construction Ordinance. Public Works Standard Construction Measure 9 (Cultural Resources) would require the project to implement the following measures to protect in place the two existing Path of Gold Light Standards in the project vicinity during construction activities:

- The project plan set must identify each Path of Gold Light Standard as requiring physical protection,
- The project plan set must establish environmentally sensitive areas (ESA) radii around each Path of Gold Light Standard (see Figure 3 for details),
- A plan sheet showing the detail of the required physical protection of each Path of Gold Light Standard must be included in the plan set (see Figure 3 for details), and
- The required physical protection of the Path of Gold Light Standards must be constructed and installed to the satisfaction of the Public Works Regulatory Affairs representative overseeing implementation of Public Works Standard Construction Measure 9 (Cultural Resources) before construction activities in the immediate vicinity of the Path of Gold Light Standards may commence.

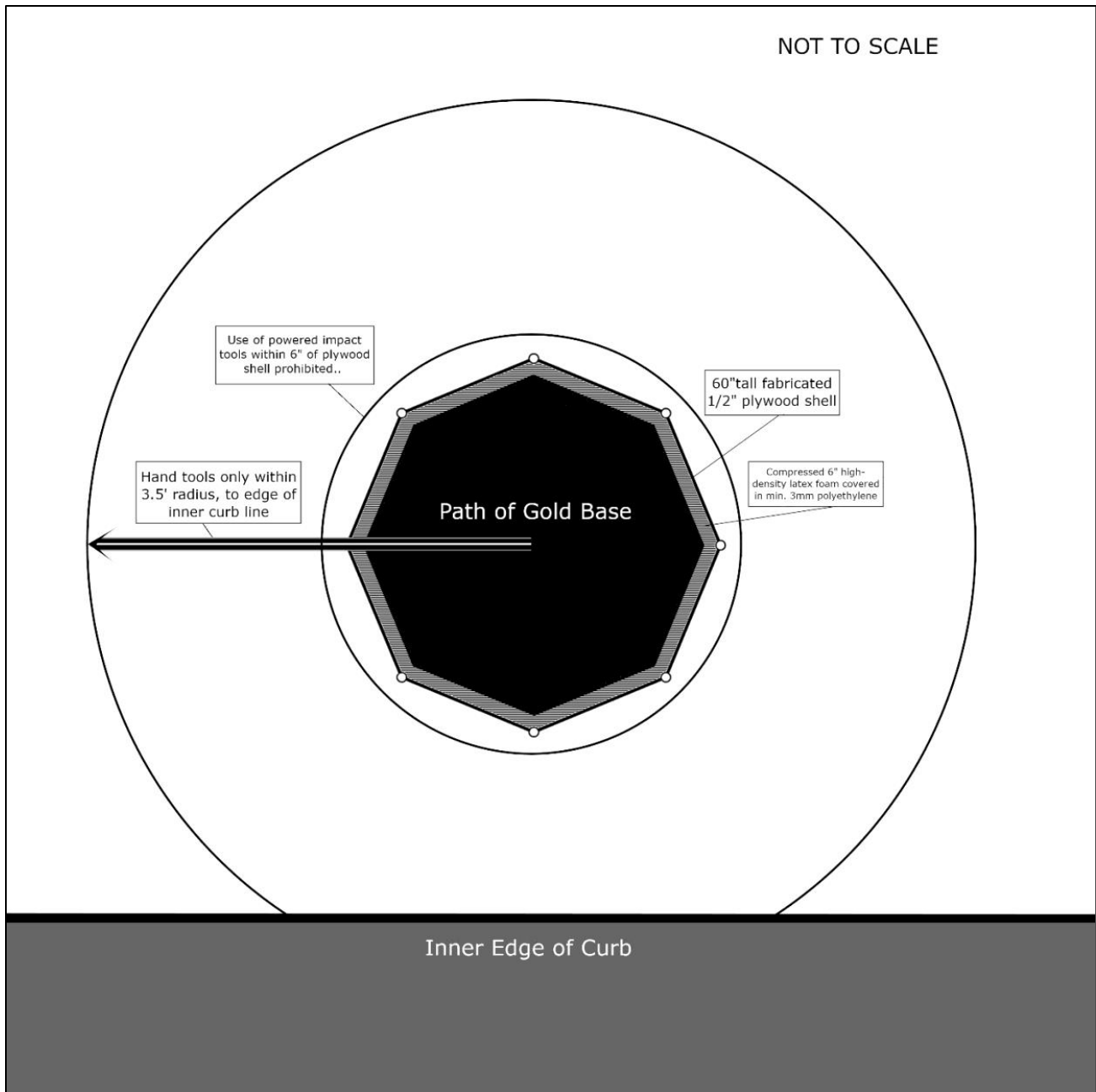


Figure 3. Illustration of Required Physical Protection for Path of Gold Light Standards.

Planned Projects in the Vicinity

Upper Market Street Safety Project

The Upper Market Street Safety Project (2017-000817ENV) would implement pedestrian and bicycle safety improvements along Market Street from Castro Street to Octavia Boulevard. The project would include new curb extensions at several locations designed to shorten crossing distances and increase the visibility of people entering crosswalks, and related transit island and pedestrian refuge

improvements. The project would also include parking-protected (Class IV) bike lanes, and changes to traffic circulation to increase safety for people using all modes of transportation. Project construction would include ADA-compliant curb ramps, the possible relocation of catch basins or manholes, and the possible replacement of pull boxes. The project is currently under construction.

New Harvey Milk Plaza Project

The New Harvey Milk Plaza Project (2019-013425ENV) would renovate the existing Harvey Milk Plaza and southern entrance to Castro Muni Station. The existing site would be regraded, filled where necessary, and re-surfaced. The existing curvilinear planters and retaining wall along the northern edge of the site would be removed and a new retaining wall would be installed, running along the northern edge of the site, starting near Castro Street and ending at the Western edge of the new elevator glass enclosure. The Castro Station Accessibility project would not conflict with or preclude the construction of the new plaza.

Approval Action

The first approval of the project committing the City to carrying out the proposed project would be BART's issuance of a building or site permit.

Attachments

Attachment A – Castro Station Elevator Plans

Attachment B – Castro Station Accessibility Project Presentation from Friends of Harvey Milk Plaza Meeting, January 11, 2021

Attachment C – Castro Station Elevator Existing Conditions and Landscape Demolition Plan (Street Level) and Materials and Layout Plan (Street Level)

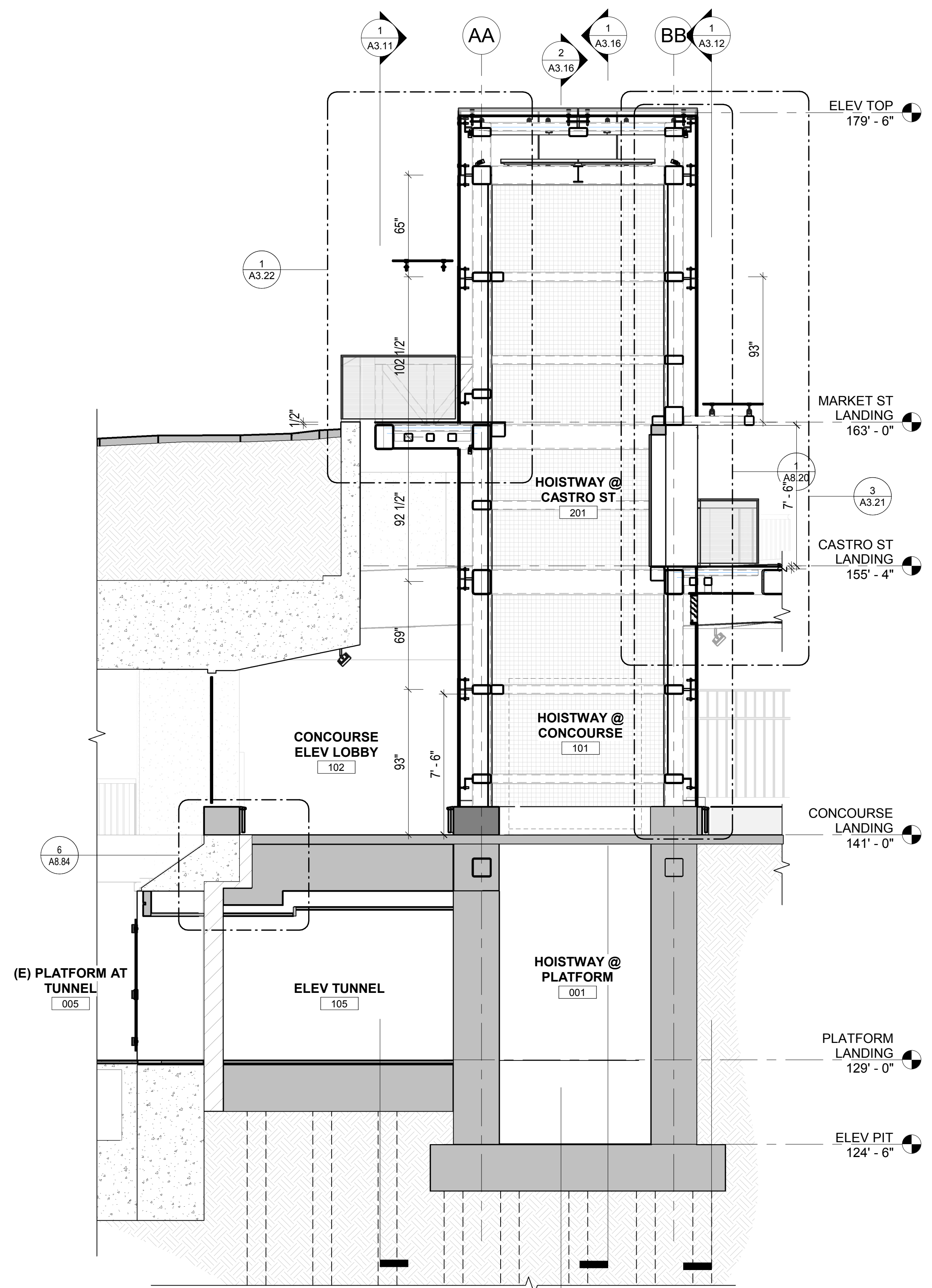


Ron Alameida - City Architect & Deputy Director
 Julia Laue - Principal Architect / Bureau Manager
 49 South Van Ness Ave., Suite # 1100
 San Francisco CA 94103
 Phone: (628)271.2865

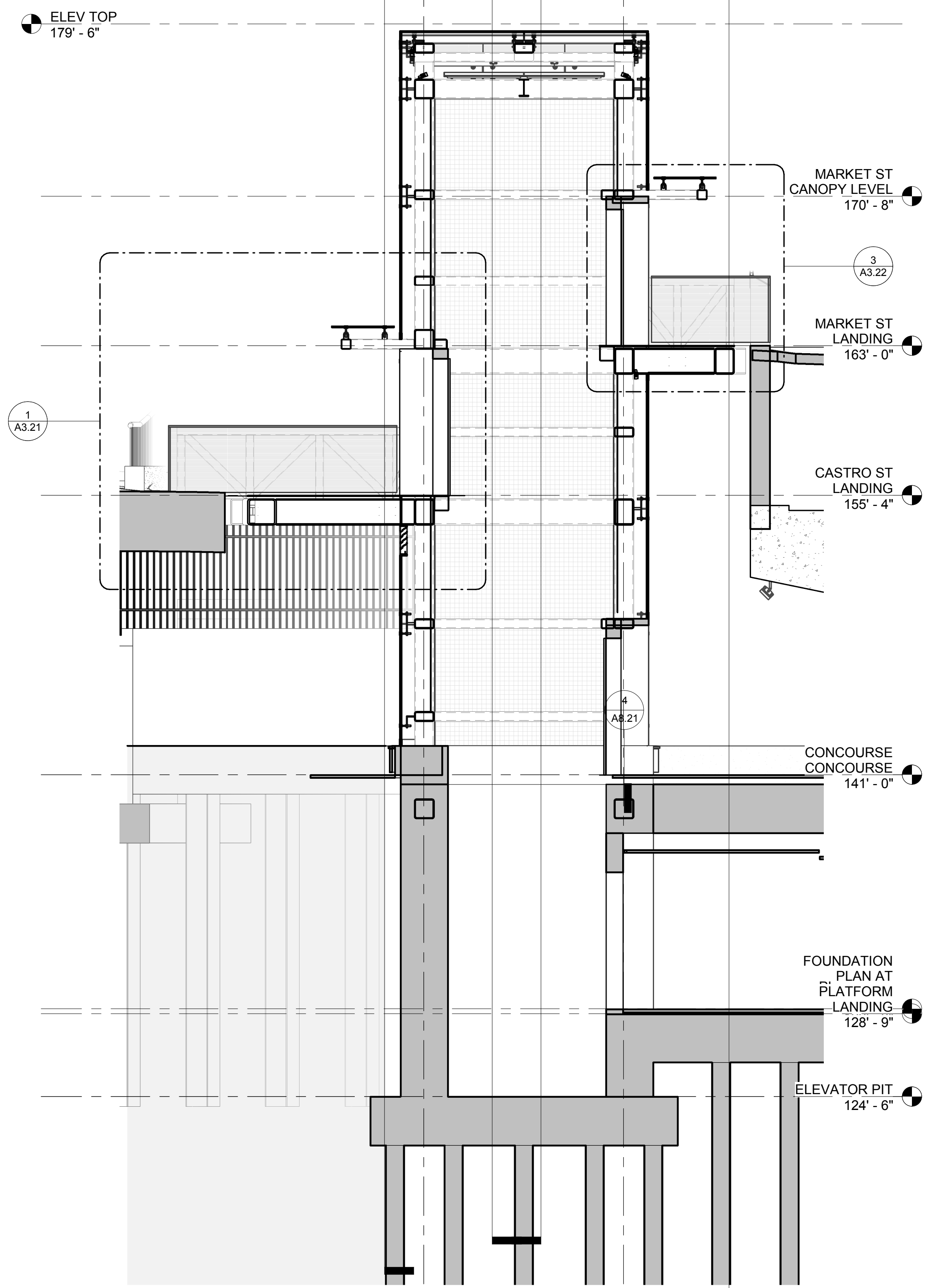
CASTRO STATION ELEVATOR

CASTRO MUNI SUBWAY STATION
 CASTRO STREET & MARKET STREET
 SAN FRANCISCO, CA, 94114

Consultant



2 SEC_A-A_THRU HOISTWAY_EAST
 1/4" = 1'-0"



1 SECTION A-A THRU HOISTWAY - WEST
 1/4" = 1'-0"

50% CD 1/22/2021

No.	Date	Revisions

PM T. KAVANAGH
 PA J. CHAN
 Drawn TEAM
 Checked Checker

NOT FOR CONSTRUCTION

Drawing Title

ELEVATOR SECTIONS THRU HOISTWAY

Sheet No. **A3.15**

Scale: 1/4" = 1'-0"
 Project No. PW-MTA-1901

C:\Users\lanChan\Documents\PW_MTA_1901_CASTRO_ELEV_ARCH_R20_lanChan.rvt 2/26/2021 10:45:12 AM

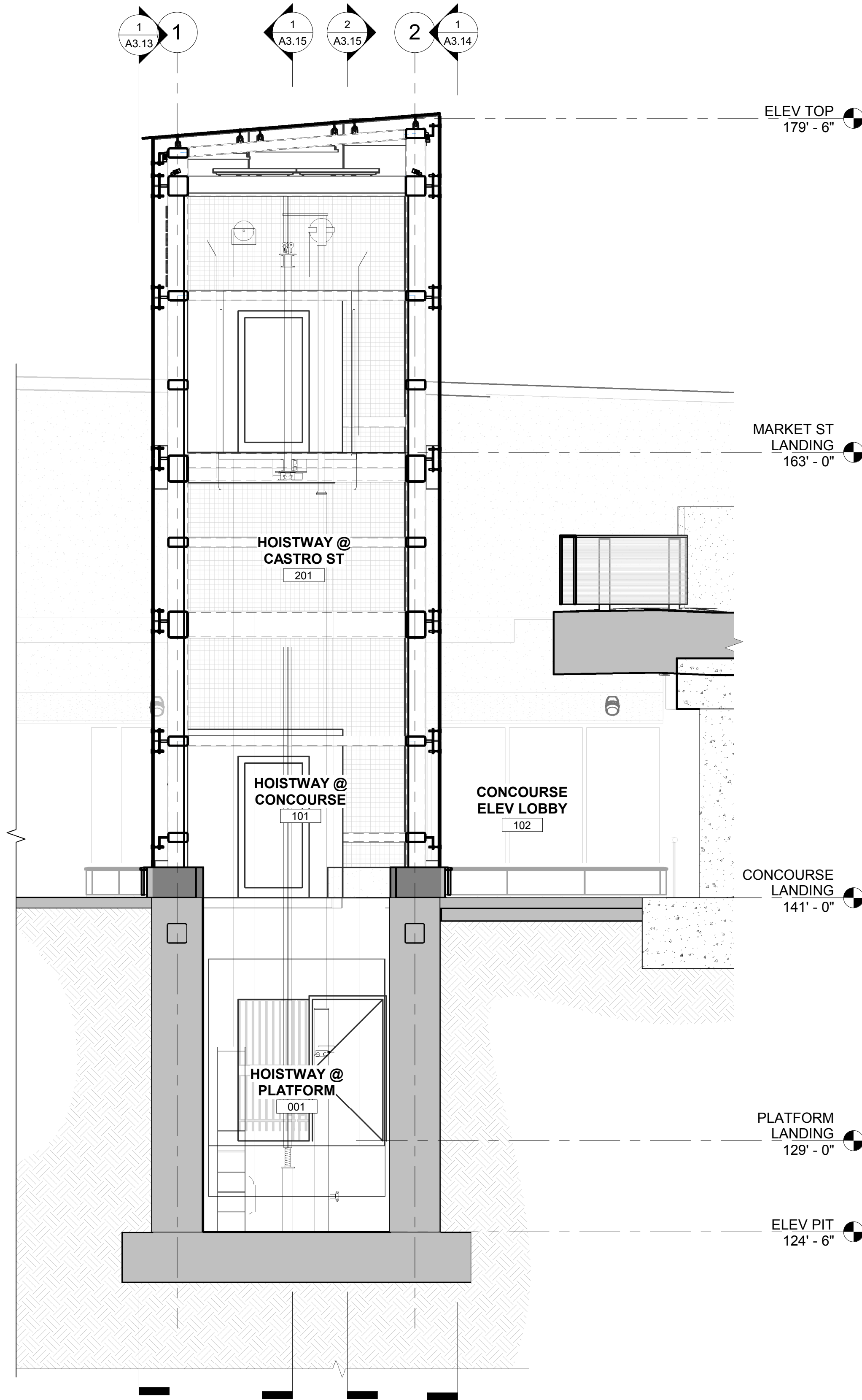


Ron Alameida - City Architect & Deputy Director
 Julia Laue - Principal Architect / Bureau Manager
 49 South Van Ness Ave., Suite # 1100
 San Francisco CA 94103
 Phone: (628)271.2865

CASTRO STATION ELEVATOR

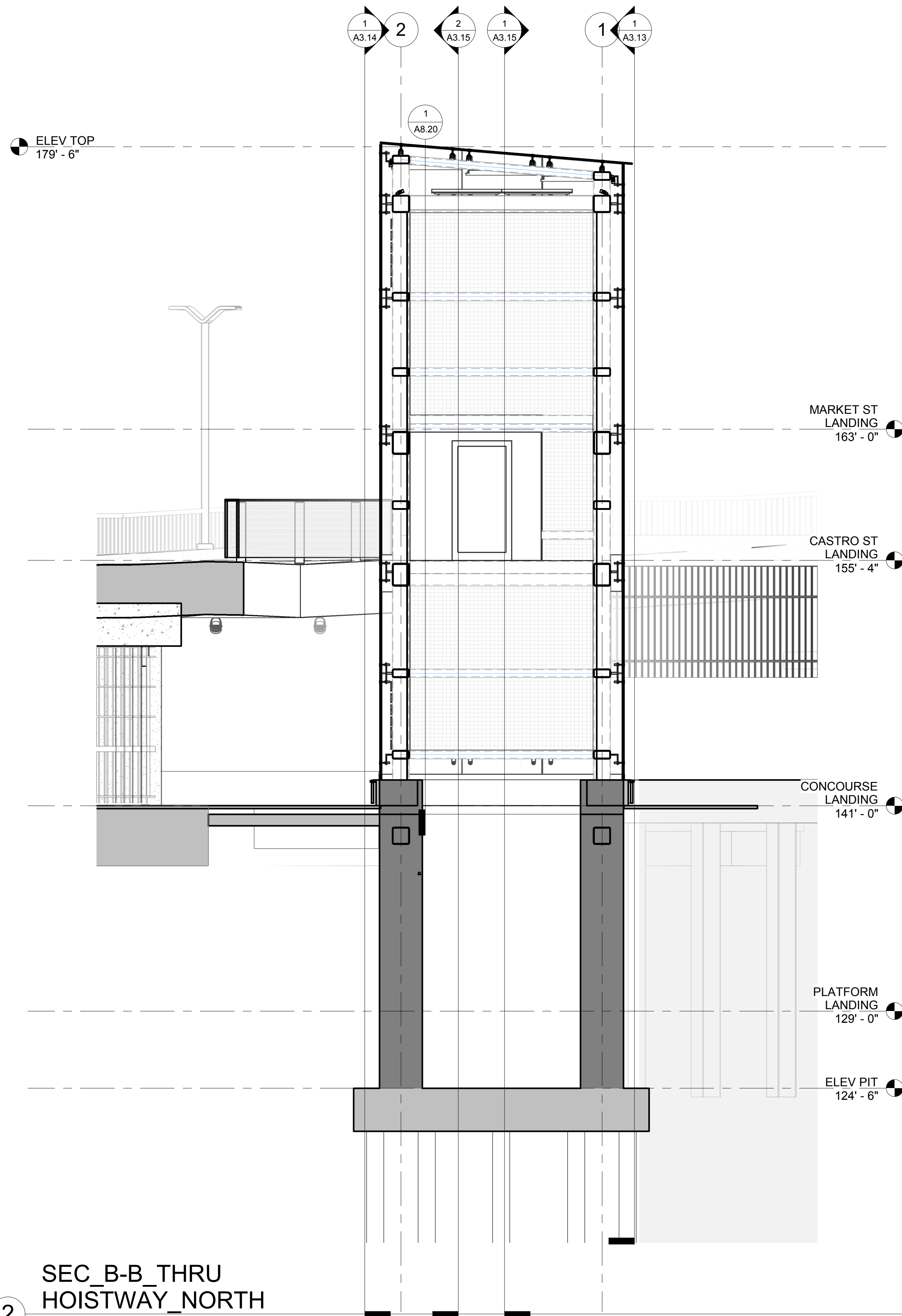
CASTRO MUNI SUBWAY STATION
 CASTRO STREET & MARKET STREET
 SAN FRANCISCO, CA, 94114

Consultant



**SEC_B-B_THRU
 HOISTWAY_SOUTH**

1/4" = 1'-0"



**SEC_B-B_THRU
 HOISTWAY_NORTH**

1/4" = 1'-0"

50% CD 1/22/2021

No.	Date	Revisions

PM T. KAVANAGH
 PA J. CHAN
 Drawn TEAM
 Checked Checker



Drawing Title PROJECT ARCHITECT JANE CHAN

**ELEVATOR SECTIONS THRU
 HOISTWAY**

Sheet No. **A3.16**

Scale: 1/4" = 1'-0"

Project No. PW-MTA-1901

C:\Users\JanChan\Documents\PW_MTA_1901_CASTRO_ELEV_ARCH_R20_JanChan.rvt 2/26/2021 10:45:16 AM



Ron Alameida - City Architect & Deputy Director
 Julia Laue - Principal Architect / Bureau Manager
 49 South Van Ness Ave., Suite # 1100
 San Francisco CA 94103
 Phone: (628)271.2865

CASTRO STATION ELEVATOR

CASTRO MUNI SUBWAY STATION
 CASTRO STREET & MARKET STREET
 SAN FRANCISCO, CA, 94114

Consultant

GENERAL NOTES

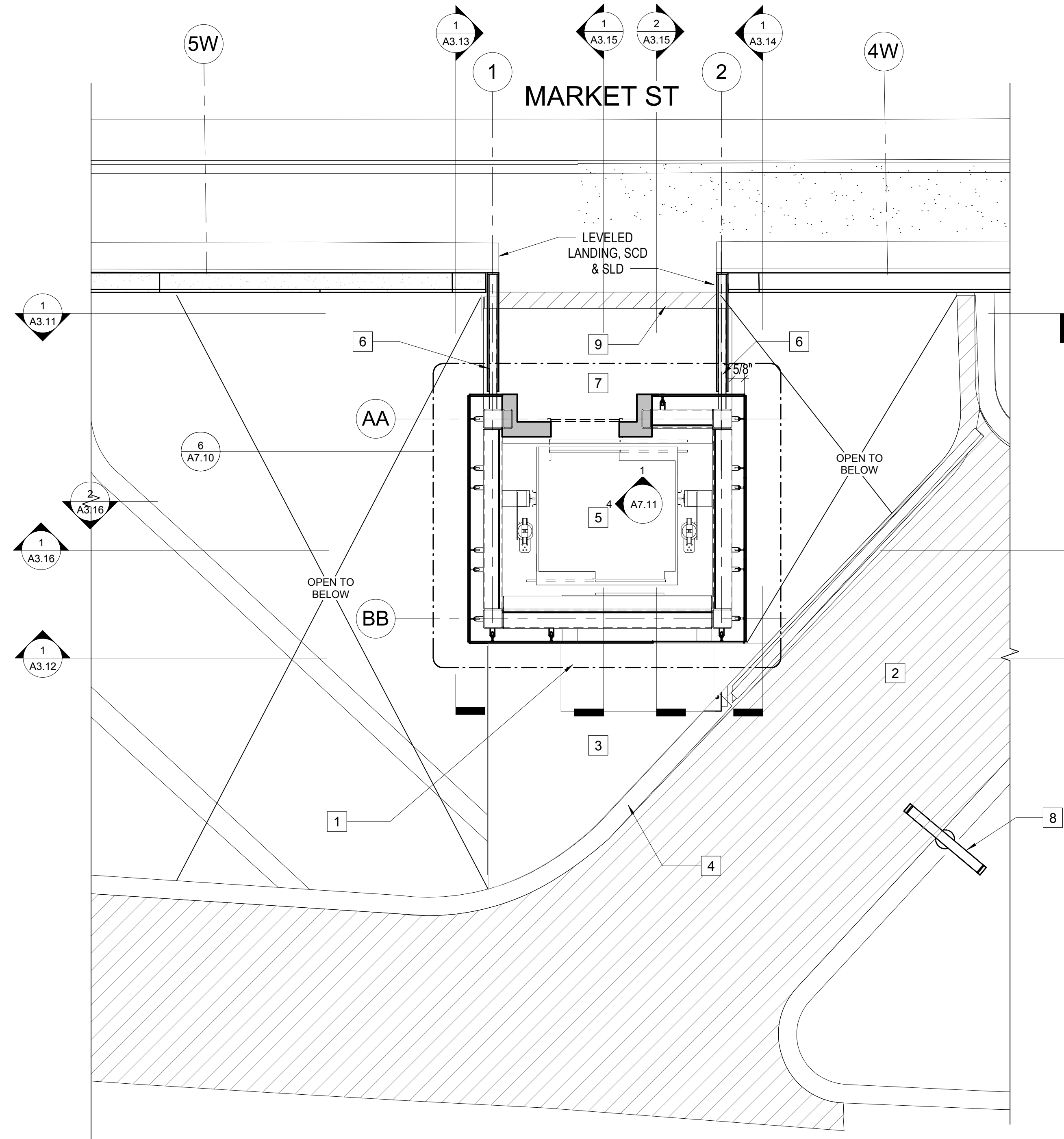
- 1 SEE THE ELECTRICAL DRAWINGS FOR LOCATIONS OF RELATED POWER & COMMUNICATION SYSTEMS OUTLET LOCATIONS. COORDINATE WITH ARCHITECT IN FIELD
- 2 SEE ENLARGED ARCH PLANS FOR ELEVATOR AND BRIDGE DESIGN
- 3 GRID LINES ARE FACE TO CENTER OF COLUMNS. SSD
- 4 SEE G- SHTS FOR ADA REQUIREMENTS
- 5 SEE CIVIL & LANDSCAPE PLANS FOR FULL SITE IMPROVEMENT SCOPE INCLUDING STREETScape AND CURB RAMPS
- 6 SEE G2.30, G2.31 AND G2.32 FOR CONSTRUCTION ACCESS & FIRE EGRESS
- 7 SEE A9.10 FOR PARTITION TYPES & PARTITION DETAILS

SHEET NOTES

- 1 (N) CANOPY AT LEVEL BELOW
- 2 (N) PAVING, SLD
- 3 (N) PLATFORM BRIDGE BELOW TO CONNECT PEDESTRIAN PATH & ELEVATOR
- 4 10" SEISMIC JOINT & COVER @ BRIDGE & PEDESTRIAN PATH, SEE DETS & SSD
- 5 (N) ELEVATOR, SEE DETAILS AND SSD
- 6 (N) MTL PANELS OVER STRUCT TRUSS GUARDRAIL AT BRIDGE, SEE DETAILS & SSD
- 7 (N) PLATFORM BRIDGE TO CONNECT MARKET ST SIDEWALK & ELEVATOR
- 8 (N) PEDESTRIAN PATH LIGHT FIX ON (N) MTL POLE, SLD & SED
- 9 (N) SEISMIC EXP JOINT, SEE DETS & SSD

LEGEND

- (N) WALL, SEE WALL TYPES & FINISH SCH TYP.
- (E) CUT CONCRETE WALL
- (E) EXTERIOR & INTERIOR WALL TO REMAIN
- (N) PAVING, SLD
- # SHEET NOTES



1 MARKET ST LANDING - PLAN
 1/4" = 1'-0"

50% CD 1/22/2021

No.	Date	Revisions

PM T. KAVANAGH
 PA J. CHAN
 Drawn TEAM
 Checked Checker

NOT FOR CONSTRUCTION

Drawing Title

MARKET ST LANDING ENLARGED FLOOR PLAN

Sheet No. **A4.10**

Scale: As indicated

Project No. PW-MTA-1901



Ron Alameida - City Architect & Deputy Director
 Julia Laue - Principal Architect / Bureau Manager
 49 South Van Ness Ave., Suite # 1100
 San Francisco CA 94103
 Phone: (628)271.2865

CASTRO STATION ELEVATOR

CASTRO MUNI SUBWAY STATION
 CASTRO STREET & MARKET STREET
 SAN FRANCISCO, CA, 94114

Consultant

GENERAL NOTES

- 1 SEE THE ELECTRICAL DRAWINGS FOR LOCATIONS OF RELATED POWER & COMMUNICATION SYSTEMS OUTLET LOCATIONS. COORDINATE WITH ARCHITECT IN FIELD
- 2 SEE ENLARGED ARCH PLANS FOR ELEVATOR AND BRIDGE DESIGN
- 3 GRID LINES ARE FACE TO CENTER OF COLUMNS. SSD
- 4 SEE G- SHTS FOR ADA REQUIREMENTS
- 5 SEE CIVIL & LANDSCAPE PLANS FOR FULL SITE IMPROVEMENT SCOPE INCLUDING STREETScape AND CURB RAMPS
- 6 SEE G2.30, G2.31 AND G2.32 FOR CONSTRUCTION ACCESS & FIRE EGRESS
- 7 SEE A9.10 FOR PARTITION TYPES & PARTITION DETAILS

SHEET NOTES

- 1 (N) BRIDGE GUARDRAIL, SEE DETAILS ON A 8.80
- 1 MTL PANELS OVER STRUCT TRUSS GUARDRAIL @ BRIDGE, SEE DETS & SSD
- 2 (N) PAVING, SLD
- 3 10" SEISMIC JOINT & COVER @ BRIDGE & PEDESTRIAN PATH, SEE DETS & SSD
- 4 (N) POINT SUPPORT GLASS CANOPY, SEE DETS & SSD
- 5 (N) ELEVATOR, SEE DETAILS AND SSD
- 6 (N) PLATFORM BRIDGE TO CONNECT PEDESTRIAN PATH & ELEVATOR
- 7 (N) LIGHT FIX ON (N) MTL POLE, TOTAL OF 4, SLD & SED

LEGEND

- (N) WALL, SEE WALL TYPES & FINISH SCH TYP.
- (E) CUT CONCRETE WALL
- (E) EXTERIOR & INTERIOR WALL TO REMAIN
- (N) PAVING, SLD
- SHEET NOTES

50% CD 1/22/2021

No.	Date	Revisions

PM T. KAVANAGH
 PA J. CHAN
 Drawn TEAM
 Checked Checker

NOT FOR CONSTRUCTION

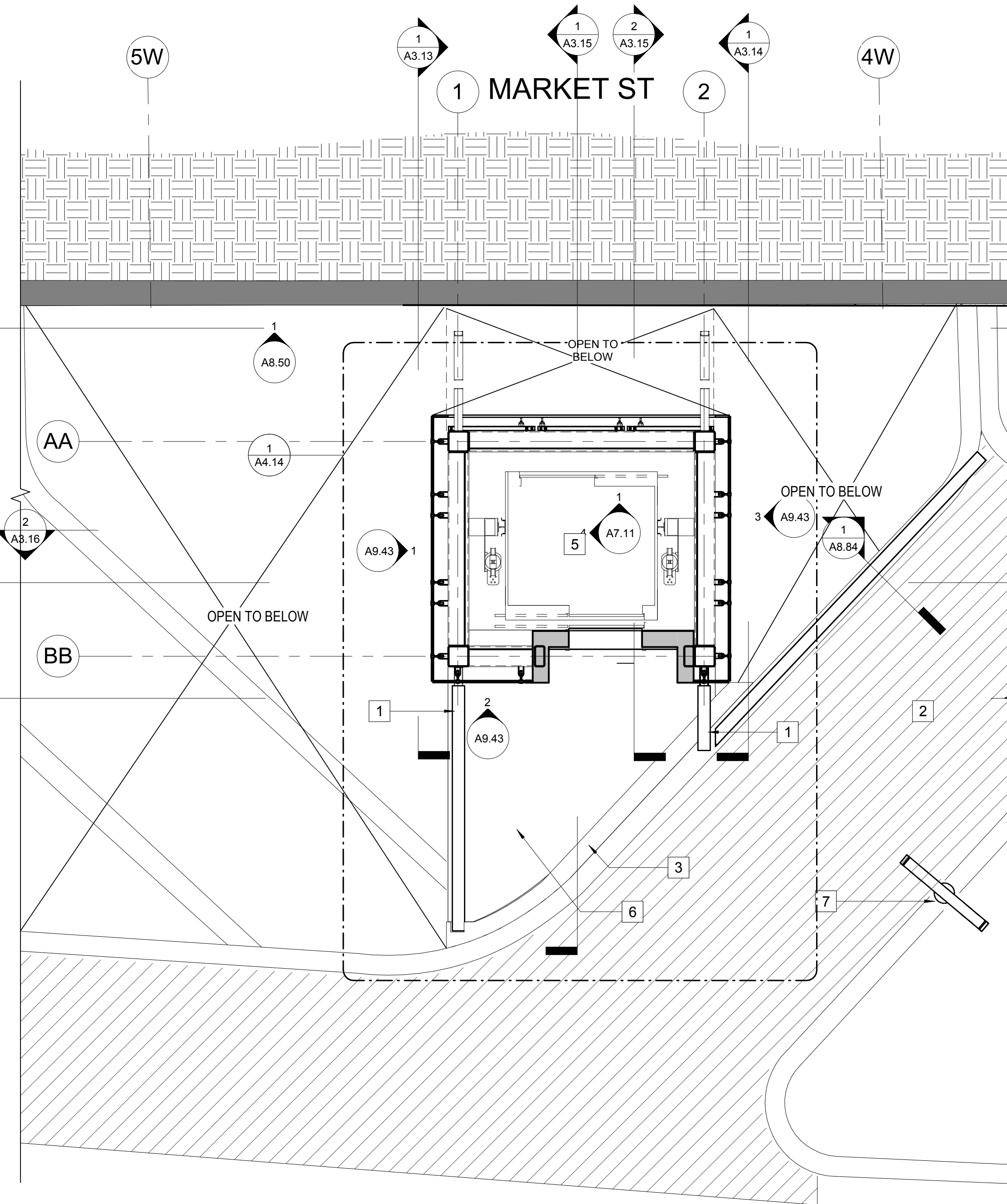
Drawing Title

CASTRO ST LANDING ENLARGED FLOOR PLAN

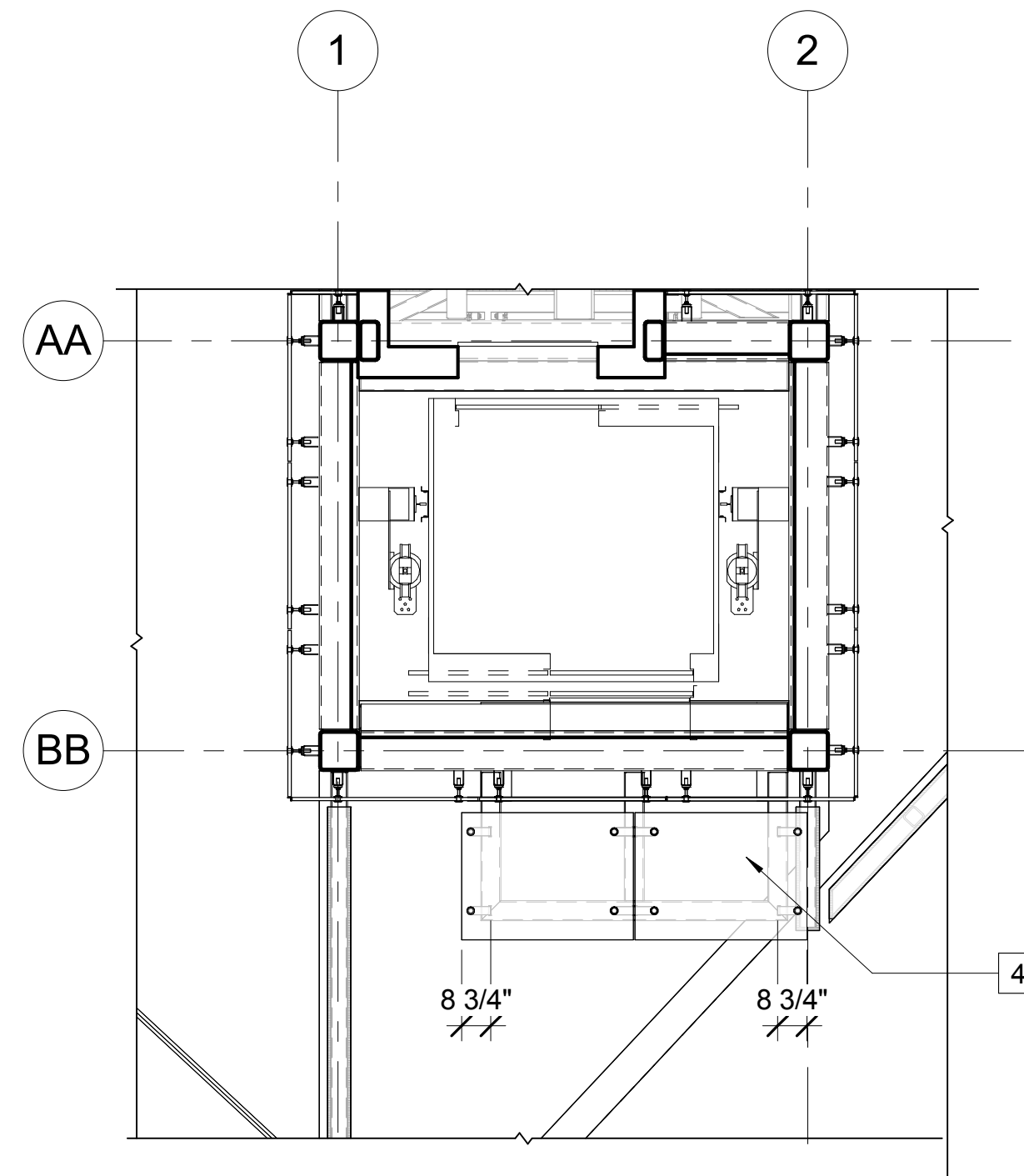
Sheet No. **A4.11**

Scale: As indicated

Project No. PW-MTA-1901



1 CASTRO ST LANDING - ENLARGED PLAN
 1/4" = 1'-0"



2 CANOPY PLAN AT CASTRO STREET LEVEL
 1/4" = 1'-0"



Ron Alameida - City Architect & Deputy Director
 Julia Laue - Principal Architect / Bureau Manager
 49 South Van Ness Ave., Suite # 1100
 San Francisco CA 94103
 Phone: (628)271.2865

CASTRO STATION ELEVATOR

CASTRO MUNI SUBWAY STATION
 CASTRO STREET & MARKET STREET
 SAN FRANCISCO, CA, 94114

Consultant

GENERAL NOTES

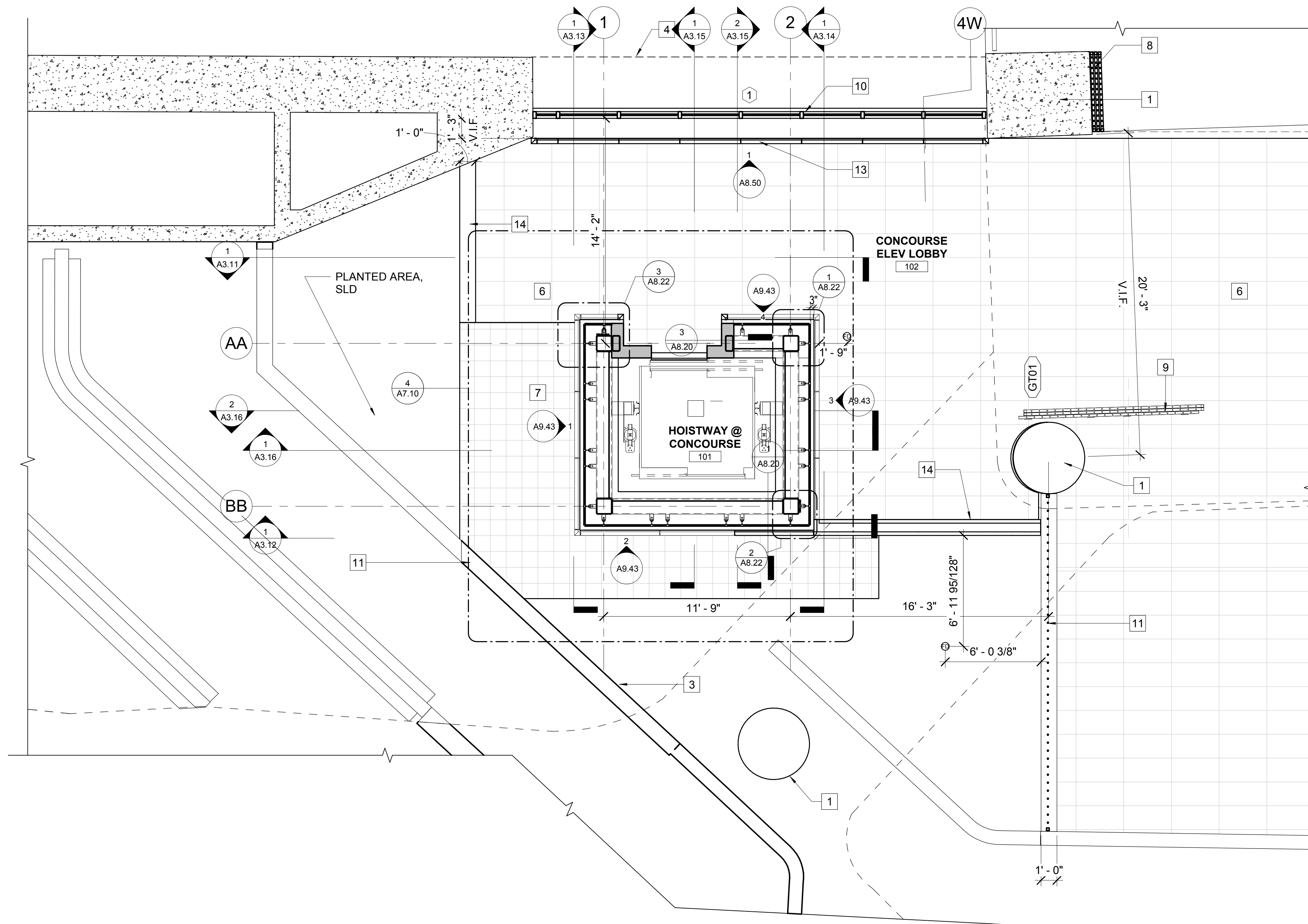
- 1 SEE THE ELECTRICAL DRAWINGS FOR LOCATIONS OF RELATED POWER & COMMUNICATION SYSTEMS OUTLET LOCATIONS. COORDINATE WITH ARCHITECT IN FIELD
- 2 SEE ENLARGED ARCH PLANS FOR ELEVATOR AND BRIDGE DESIGN
- 3 GRID LINES ARE FACE TO CENTER OF COLUMNS. SSD
- 4 SEE G- SHTS FOR ADA REQUIREMENTS
- 5 SEE CIVIL & LANDSCAPE PLANS FOR FULL SITE IMPROVEMENT SCOPE INCLUDING STREETScape AND CURB RAMPS
- 6 SEE G2.30, G2.31 AND G2.32 FOR CONSTRUCTION ACCESS & FIRE EGRESS
- 7 SEE A9.10 FOR PARTITION TYPES & PARTITION DETAILS

SHEET NOTES

- 1 (E) COL TO REMAIN
- 2 (N) ELEV CAB, SEE DETS
- 3 (N) RETAINING WALL, SSD
- 4 (E) CONC BEAM ABOVE
- 6 REPLACE (E) TERRAZZO FLG WITH (N) DECORATIVE PAVERS, AND EXTEND (N) DECORATIVE PAVERS TO ELEVATOR LOBBY, SLD
- 7 (N) NON-DECORATIVE PAVERS, SLD
- 8 (N) OPERABLE STATION SECURITY GATE TO MATCH (E)
- 9 (E) STATION SECURITY GATE TO REMAIN
- 10 (N) GLASS STOREFRONT
- 11 (N) FENCE TO MATCH (E) STATION GATE
- 13 (N) SEISMIC EXP JOINT, SEE DETS & SSD
- 14 (N) CONC CURB

LEGEND

- (N) WALL, SEE WALL TYPES & FINISH SCH TYP.
- (E) CUT CONCRETE WALL
- (E) EXTERIOR & INTERIOR WALL TO REMAIN
- (N) PAVING, SLD
- # SHEET NOTES



1 CONOURSE LANDING - ENLARGED PLAN
 1/4" = 1'-0"

50% CD 1/22/2021

No.	Date	Revisions

PM _____
 PA T. KAVANAGH
 Drawn J. CHAN
 Checked TEAM
 Checker _____

Drawing Title
CONOURSE LANDING ENLARGED FLOOR PLANS

Sheet No. **A4.12**
 Scale: As indicated
 Project No. PW-MTA-1901

NOT FOR CONSTRUCTION



Ron Alameida - City Architect & Deputy Director
 Julia Laue - Principal Architect / Bureau Manager
 49 South Van Ness Ave., Suite # 1100
 San Francisco CA 94103
 Phone: (628)271.2865

CASTRO STATION ELEVATOR

CASTRO MUNI SUBWAY STATION
 CASTRO STREET & MARKET STREET
 SAN FRANCISCO, CA, 94114

Consultant

GENERAL NOTES

- 1 SEE THE ELECTRICAL DRAWINGS FOR LOCATIONS OF RELATED POWER & COMMUNICATION SYSTEMS OUTLET LOCATIONS. COORDINATE WITH ARCHITECT IN FIELD
- 2 SEE ENLARGED ARCH PLANS FOR ELEVATOR AND BRIDGE DESIGN
- 3 GRID LINES ARE FACE TO CENTER OF COLUMNS. SSD
- 4 SEE G- SHTS FOR ADA REQUIREMENTS
- 5 SEE CIVIL & LANDSCAPE PLANS FOR FULL SITE IMPROVEMENT SCOPE INCLUDING STREETScape AND CURB RAMPS
- 6 SEE G2.30, G2.31 AND G2.32 FOR CONSTRUCTION ACCESS & FIRE EGRESS
- 7 SEE A9.10 FOR PARTITION TYPES & PARTITION DETAILS

SHEET NOTES

- 1 (N) POINT SUPPORT GLASS CANOPY, SEE DETS & SSD
- 2 (N) ELEV CAB, SEE DETS
- 3 RELOCATE (E) FHC AND PHONE CABINETS
- 4 (N) WALL & FLG W/ FINISH TO MATCH (E) STATION TILE & PATTERN. SEE PARTITION TYPES ON A9.10 & FINISH SCH
- 5 CONC FOUNDATION WALL, SSD.
- 6 (N) EXP JOINT AT FLOOR FINISH TO ALIGN WITH (E) FLOOR JOINT, SEE DETAIL 4/A9.31

50% CD 1/22/2021

No.	Date	Revisions

PM T. KAVANAGH
 PA J. CHAN
 Drawn TEAM
 Checked Checker

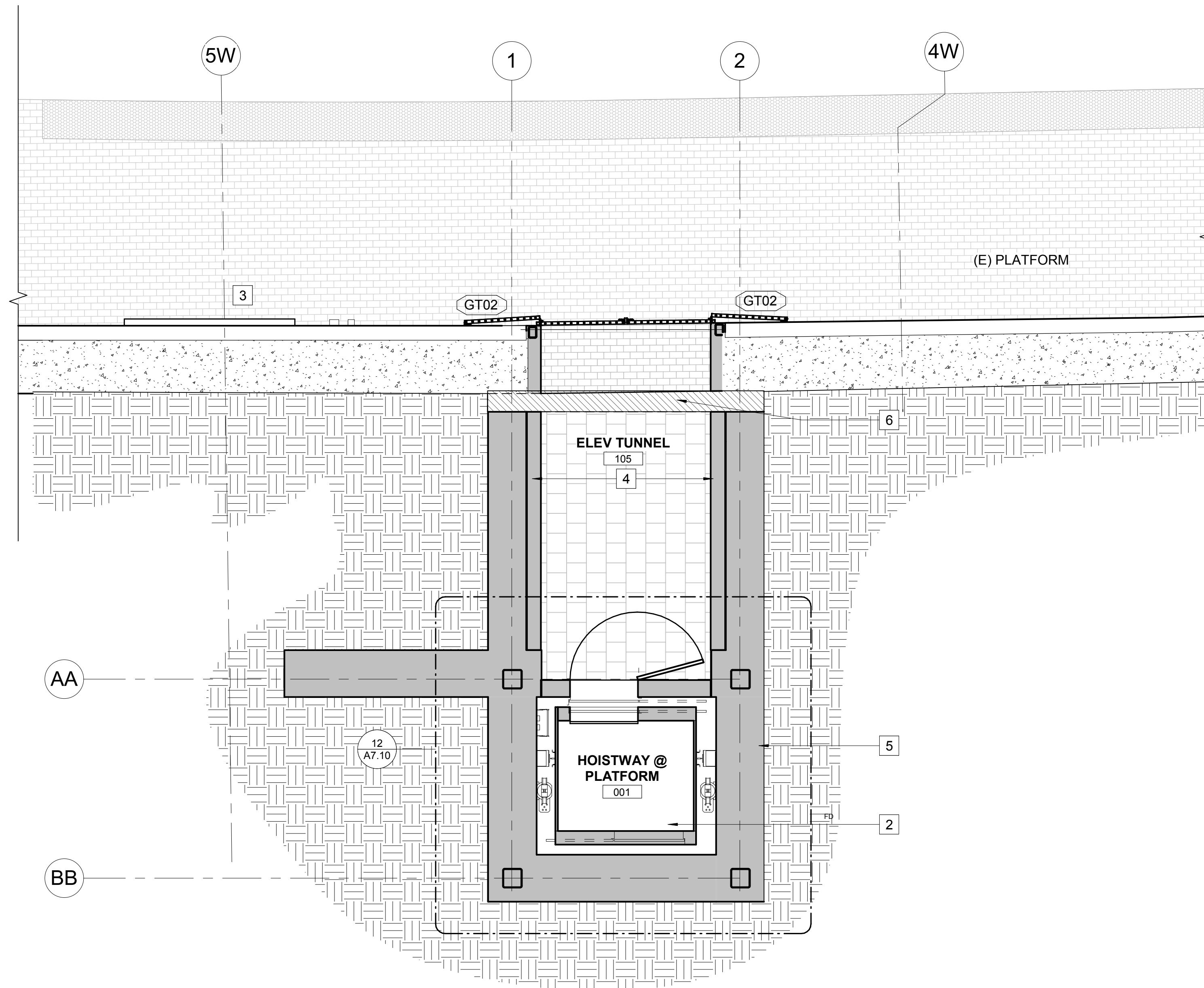
NOT FOR CONSTRUCTION

LEGEND

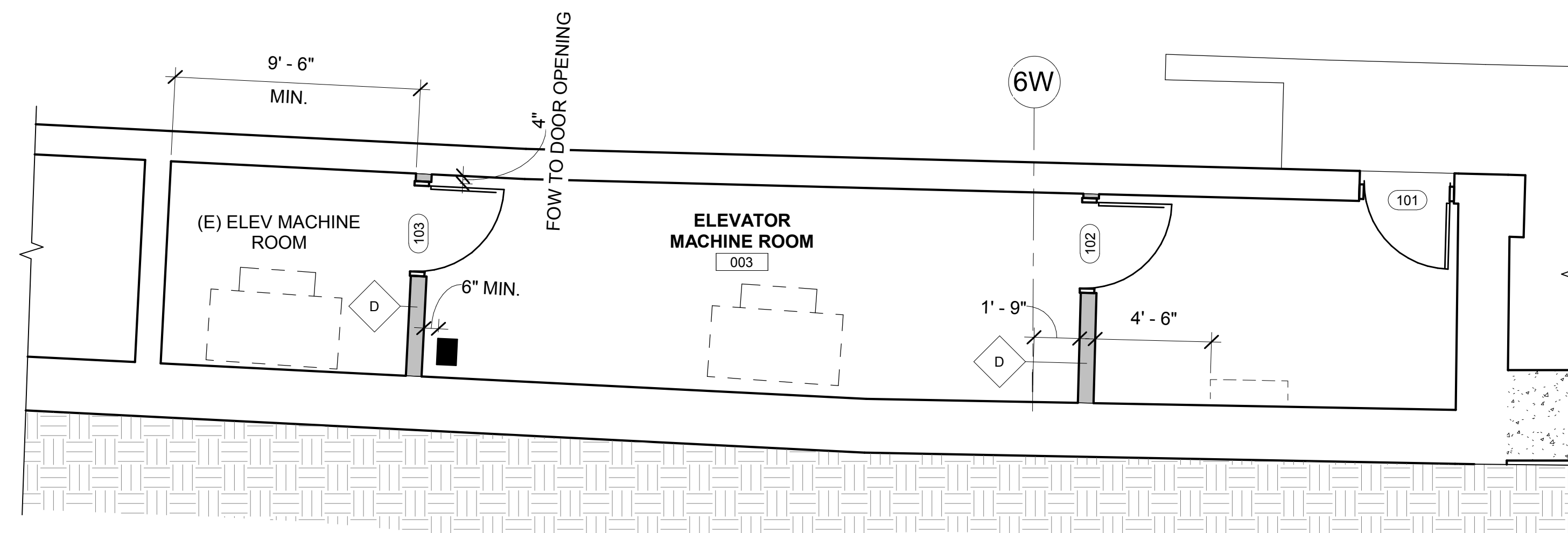
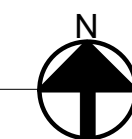
- (N) WALL, SEE WALL TYPES & FINISH SCH TYP.
- (E) CUT CONCRETE WALL
- (E) EXTERIOR & INTERIOR WALL TO REMAIN
- (N) PAVING, SLD
- SHEET NOTES

Drawing Title
PLATFORM LANDING ENLARGED FLOOR PLANS

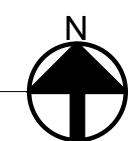
Sheet No. **A4.13**
 Scale: As indicated
 Project No. PW-MTA-1901



1 PLATFORM LANDING - (N) ELEVATOR TUNNEL - ENLARGED PLAN
 1/4" = 1'-0"



2 PLATFORM LANDING - (N) ELEVATOR MACHINE ROOM - ENLARGED PLAN
 1/4" = 1'-0"





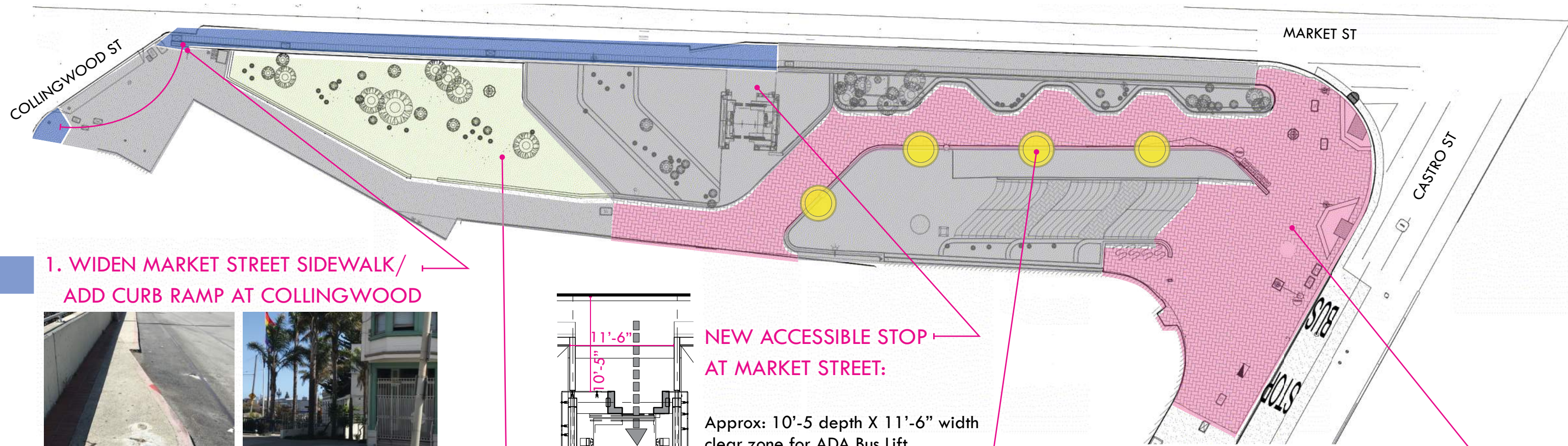
SAN FRANCISCO
PUBLIC
WORKS



CASTRO STATION ACCESSIBILITY IMPROVEMENTS

CONTENTS

1. SCOPE OF IMPROVEMENTS
2. PLANS
3. SECTION
4. RENDERINGS



**1. WIDEN MARKET STREET SIDEWALK/
ADD CURB RAMP AT COLLINGWOOD**

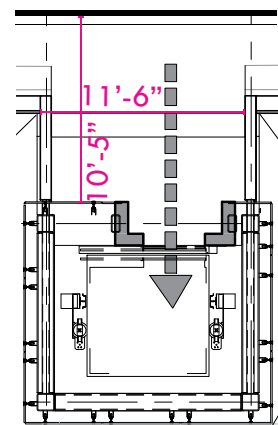


Widen Market Street Sidewalk to match adjacent width and accommodate MUNI ADA lift deployment at elevator. Add new curb ramp at Market and Collingwood.

2. REPLANT STREET LEVEL PLANTING AREA



Plant with drought tolerant and low maintenance species. Swaying plants soften the station's structure, while strategically placed agaves provide bold focal points and improve security.



**NEW ACCESSIBLE STOP
AT MARKET STREET:**

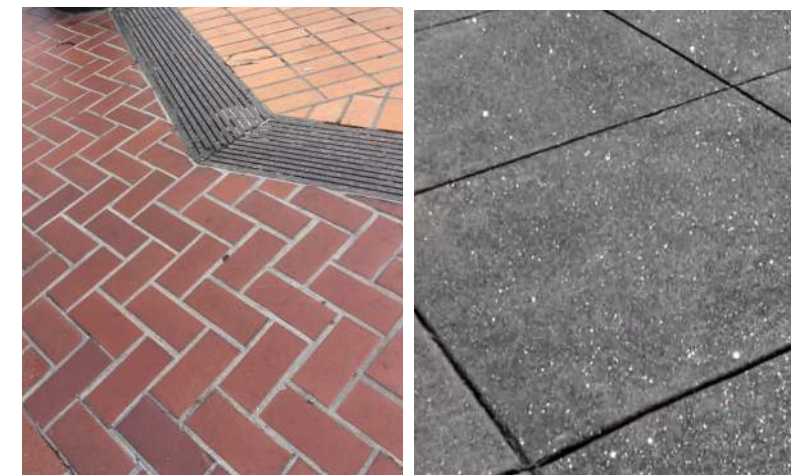
Approx: 10'-5 depth X 11'-6" width clear zone for ADA Bus Lift (8' X 8' required by SFMTA)

3. REPLACE PLAZA LIGHT FIXTURES

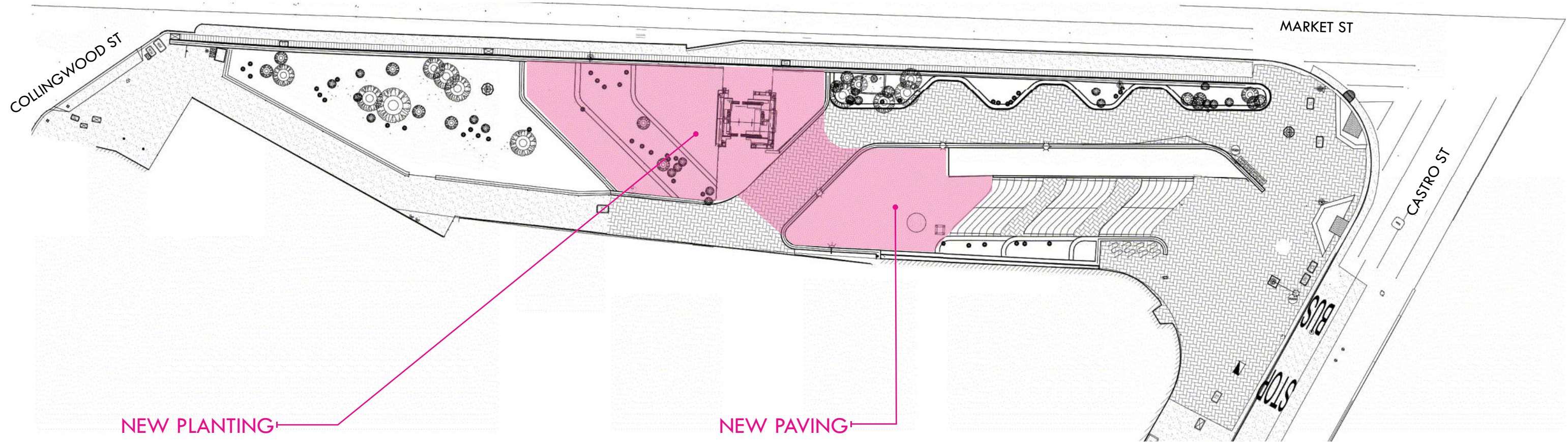


Replace existing pole top light fixtures with "Torres" fixtures by Landscape Forms.

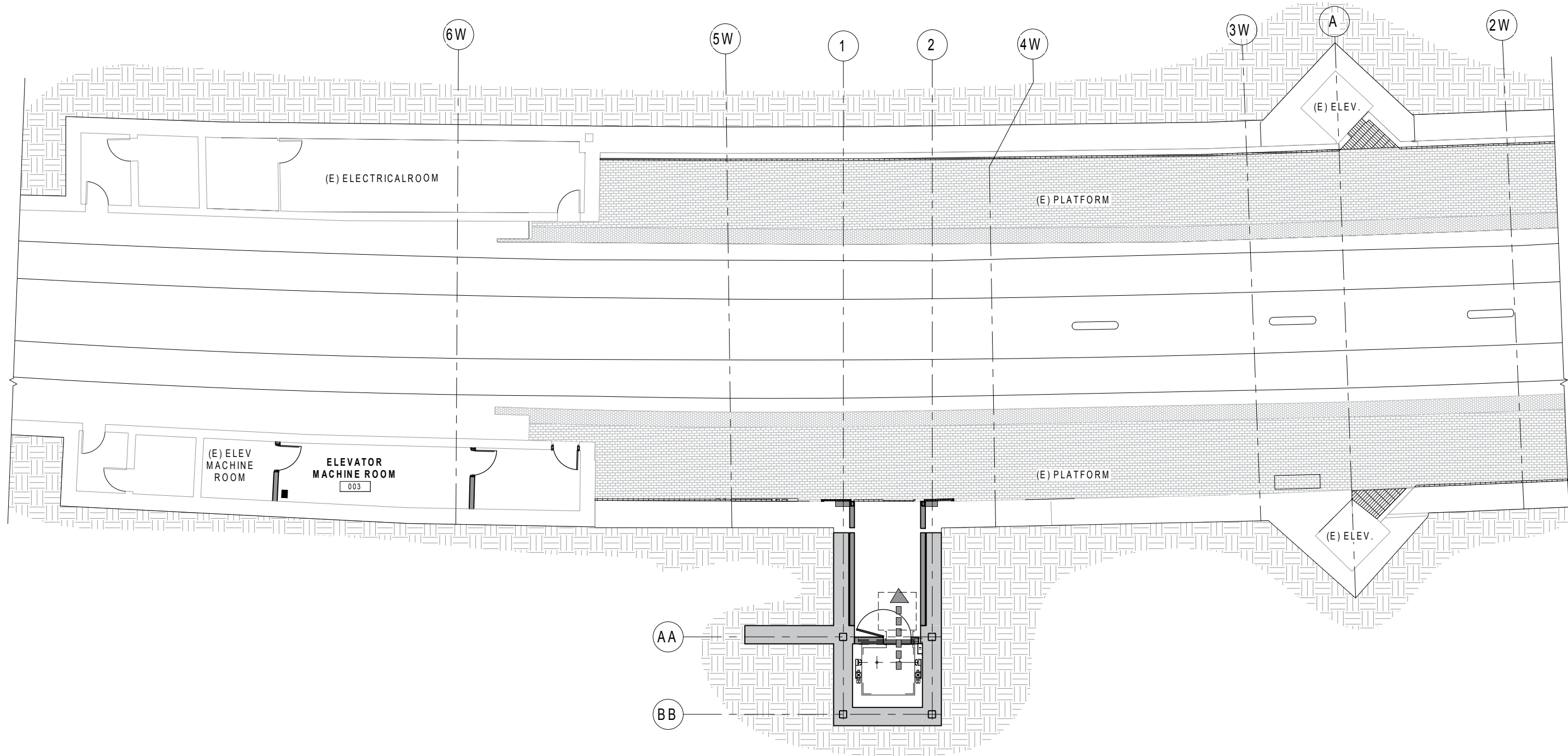
4. REGRADE AND REPLACE PLAZA PAVING



Replace existing brick with sparkle grain integral color concrete to match Castro Streetscape paving

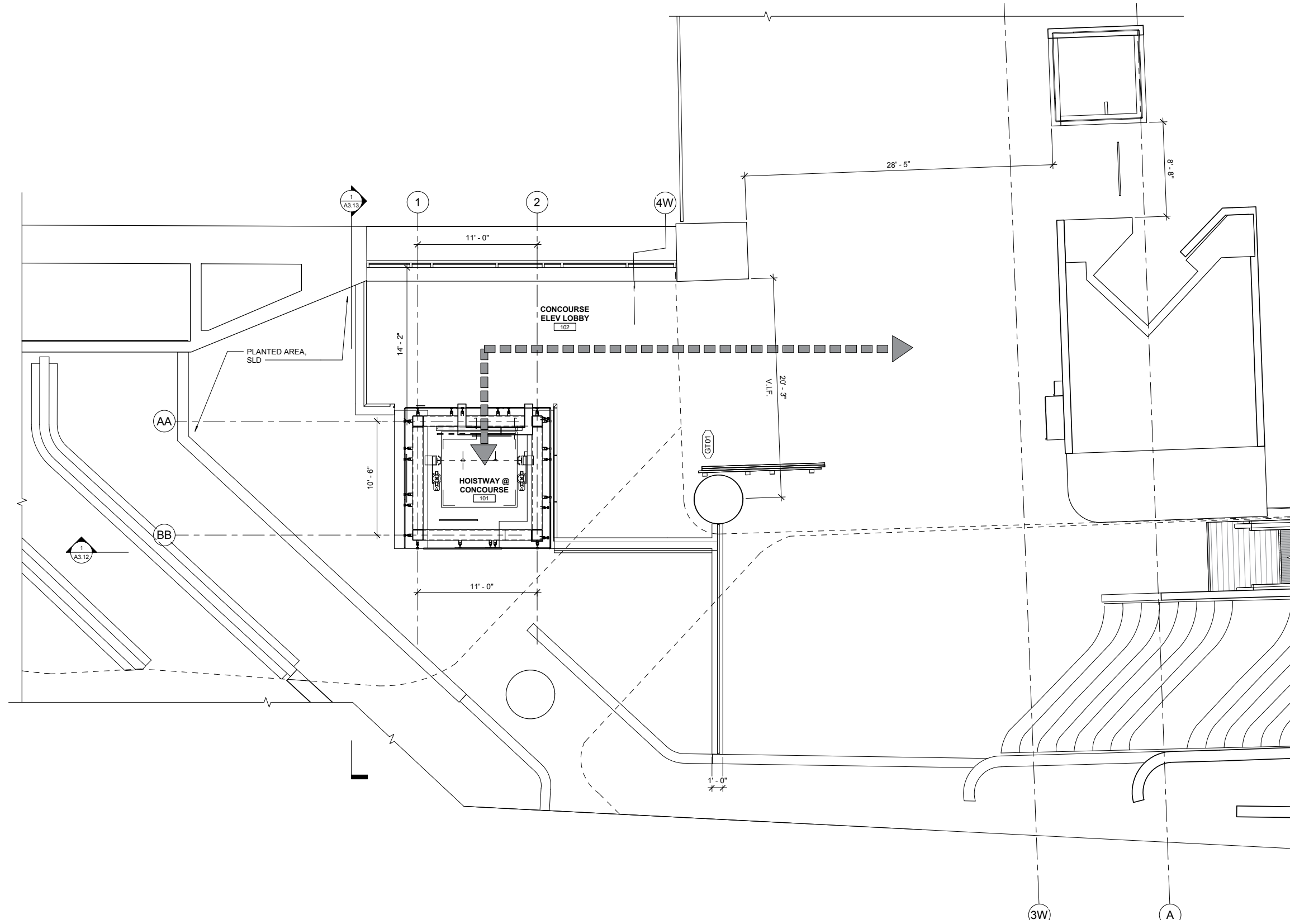


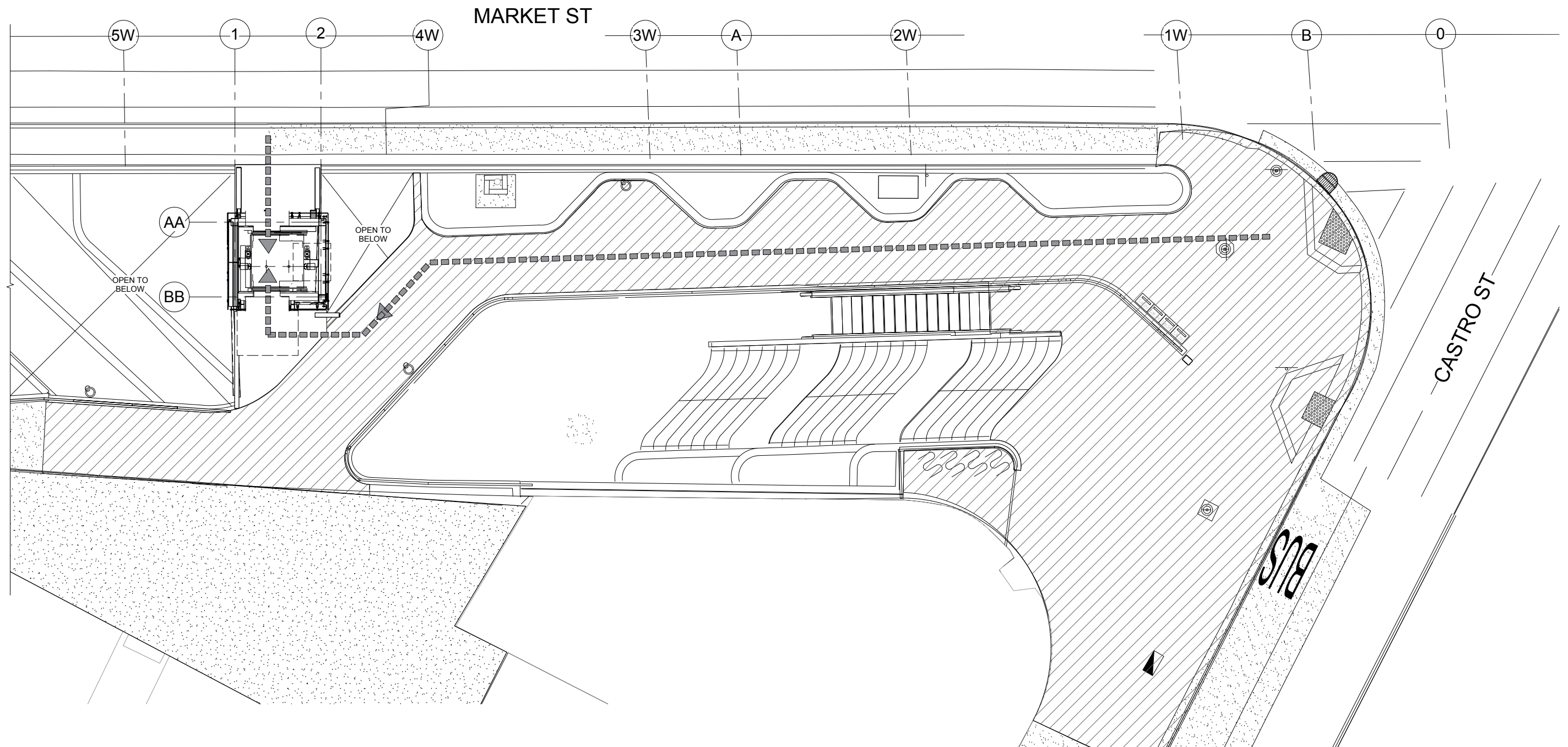
PLATFORM PLAN (not to scale)

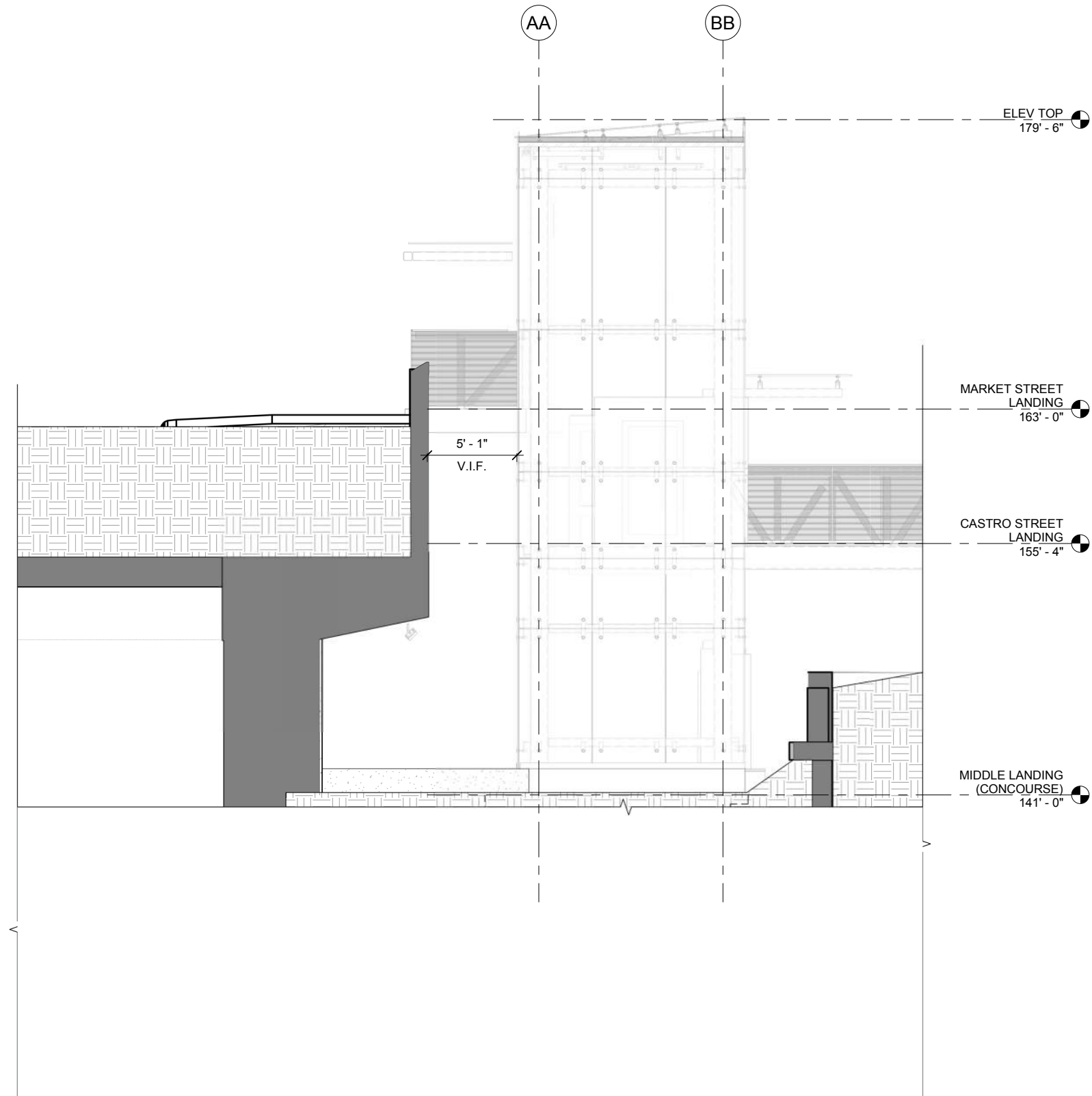


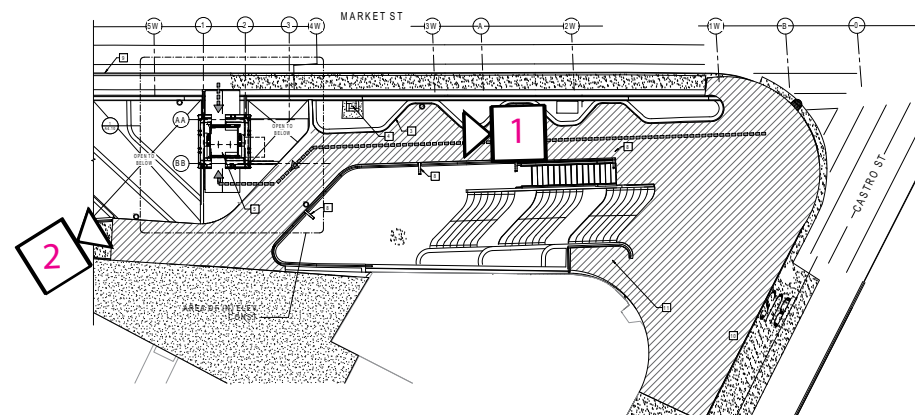
1 PLATFORM LANDING - PLAN
1/8" = 1'-0"

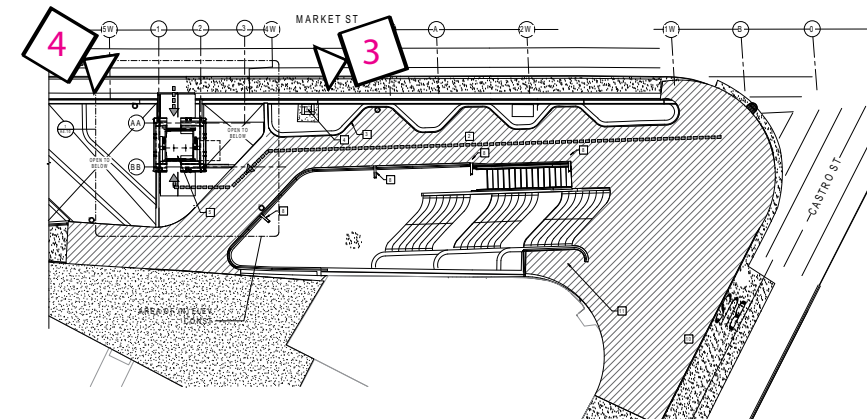














Ron Alameida - City Architect & Deputy Director
 Julia Laue - Principal Architect / Bureau Manager
 49 South Van Ness Ave., Suite # 1100
 San Francisco CA 94103
 Phone: (628)271.2865

CASTRO STATION ELEVATOR

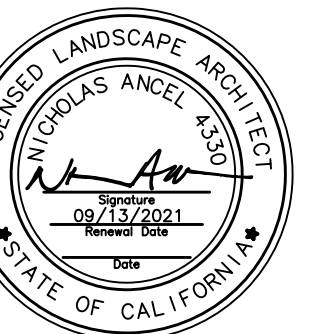
CASTRO MUNI SUBWAY STATION
 CASTRO STREET & MARKET STREET
 SAN FRANCISCO, CA, 94114

Consultant

BID SET 6/18/2021

No.	Date	Revisions

PM T. KAVANAGH
 PA J. CHAN
 Drawn L. CUEVAS
 Checked N. ANCEL

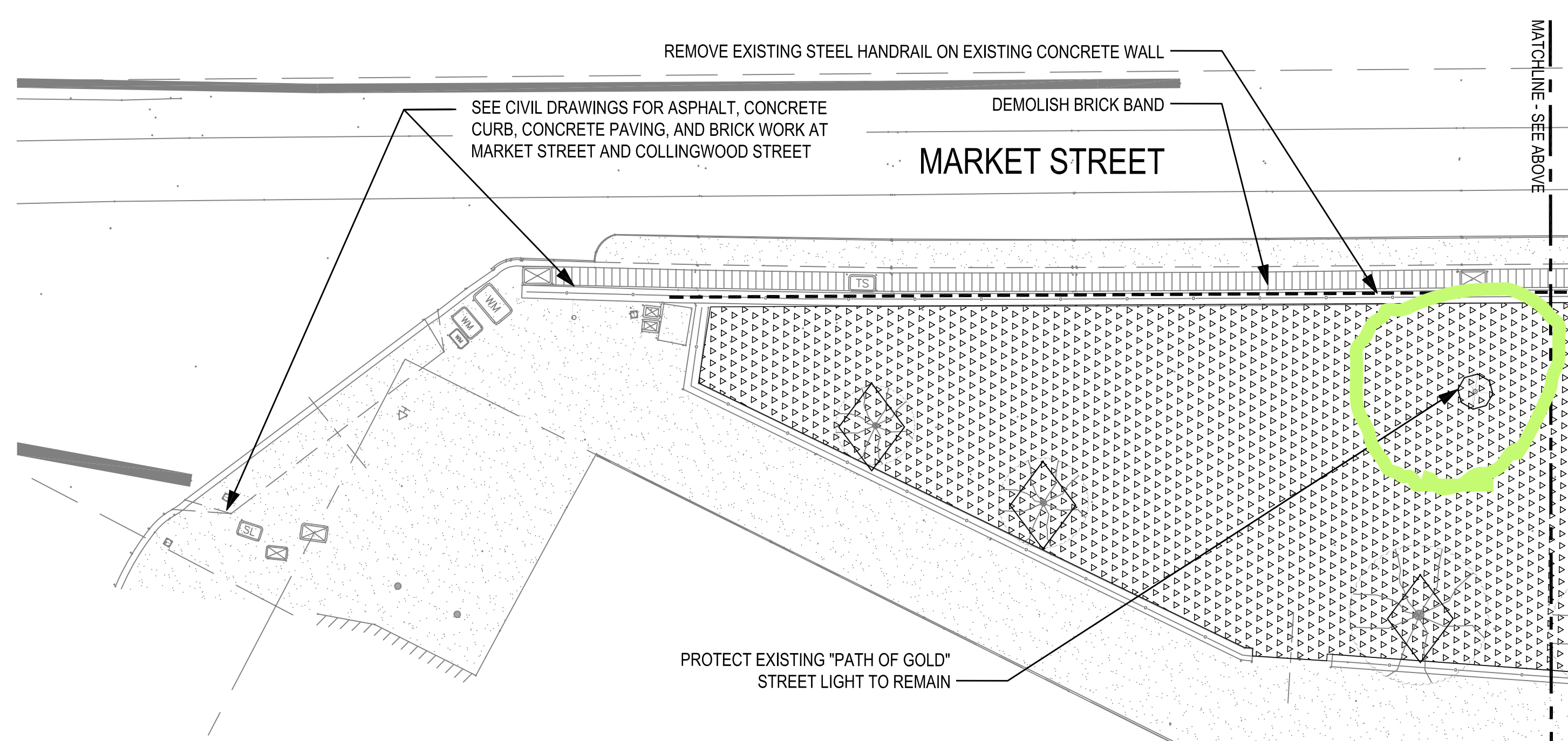
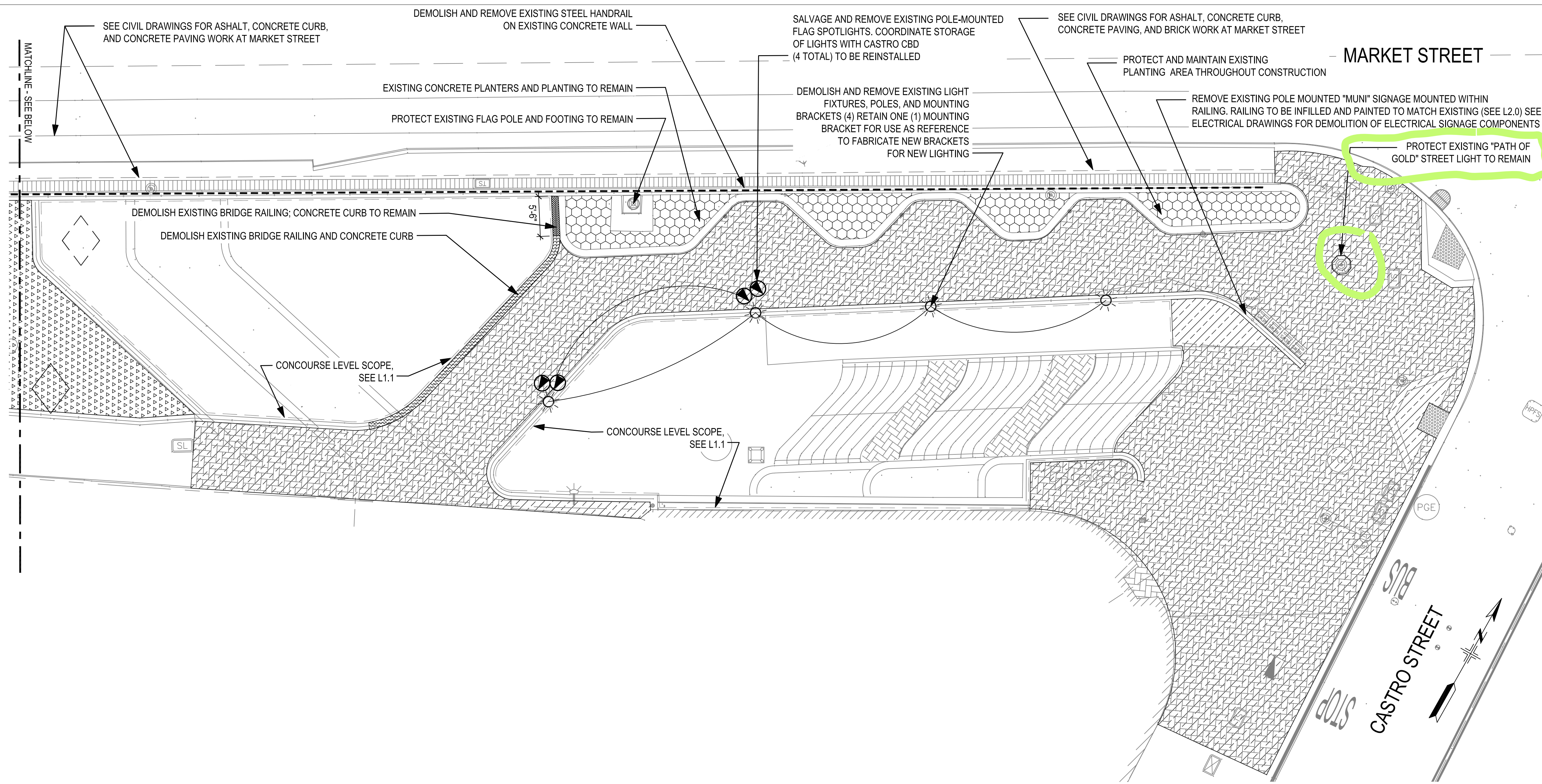


Drawing Title
EXISTING CONDITIONS & LANDSCAPE DEMOLITION PLAN - STREET LEVEL


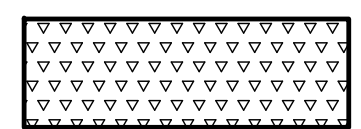
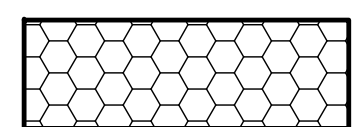
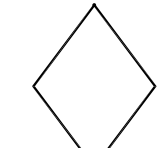


Sheet No. **L1.0**

Scale: 1/8" = 1'-0"

Project No. **PW-MTA-1901**



DEMOLITION LEGEND

-  DEMOLISH AND REMOVE EXISTING BRICK PAVING; CONCRETE BASE TO REMAIN
-  CLEAR AND GRUB EXISTING PLANTING AREA
-  PROTECT AND MAINTAIN EXISTING PLANTING AREA THROUGHOUT CONSTRUCTION
-  PROTECT EXISTING TREE TO REMAIN
-  POLE-MOUNTED FLAG UPLIGHT TO BE SALVAGED AND STORED FOR REINSTALLATION
-  PROTECT EXISTING "PATH OF GOLD" STREET LIGHT TO REMAIN



Ron Alameida - City Architect & Deputy Director
 Julia Laue - Principal Architect / Bureau Manager
 49 South Van Ness Ave., Suite # 1100
 San Francisco CA 94103
 Phone: (628)271.2865

CASTRO STATION ELEVATOR

CASTRO MUNI SUBWAY STATION
 CASTRO STREET & MARKET STREET
 SAN FRANCISCO, CA, 94114

Consultant

BID SET 6/18/2021

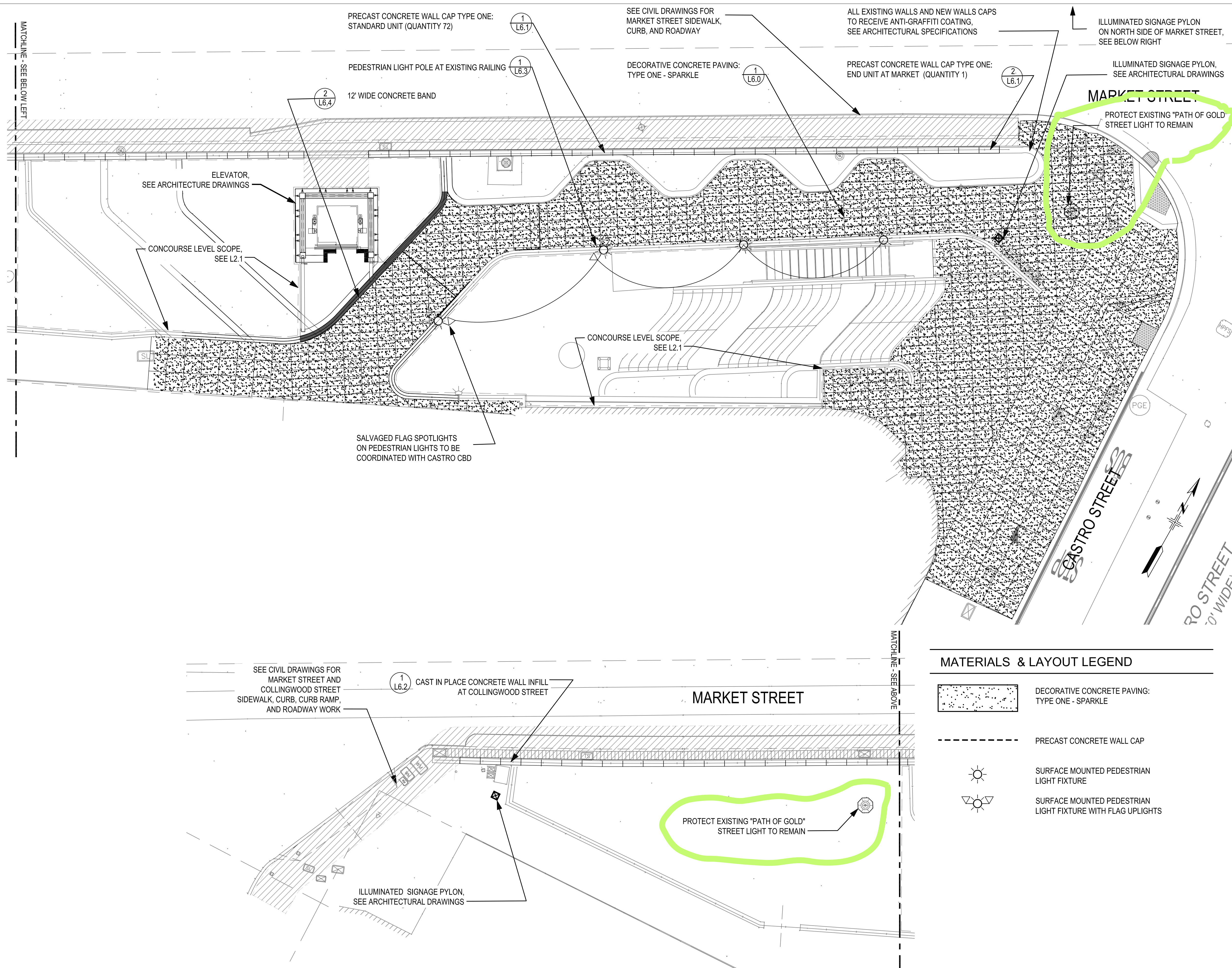
No.	Date	Revisions

PM T. KAVANAGH
 PA J. CHAN
 Drawn L. CUEVAS
 Checked N. ANCEL



Drawing Title
MATERIALS & LAYOUT PLAN - STREET LEVEL

Sheet No. **L2.0**
 Scale: 1/8" = 1'-0"
 Project No. PW-MTA-1901



MATERIALS & LAYOUT LEGEND

- DECORATIVE CONCRETE PAVING: TYPE ONE - SPARKLE
- PRECAST CONCRETE WALL CAP
- SURFACE MOUNTED PEDESTRIAN LIGHT FIXTURE
- SURFACE MOUNTED PEDESTRIAN LIGHT FIXTURE WITH FLAG UPLIGHTS

C:\Users\mmarkat\Documents\PW_MTA_1901_CASTRO_ELEV_ARCH_R20_mmarkat.rvt 6/2/2021 12:16:49 PM